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SCHEMATIC PLAN

CHESHIRE ROAD RELOCATION

Horizontal Datum
The bearings shown on this map are based on the Ohio State Plane Coordinate System, North Zone, NAD 83 (2011). Said bearings are based upon positional solutions derived from RTK GPS observations using the Ohio Department of Transportation Virtual Reference System Equipment and software. The bearing of South 86°20'30" East, assigned to the centerline of existing Cheshire Road, is designated the bases of bearings for this map.

Vertical Datum
The elevations shown on this map are based on the North American Vertical Datum of 1988. Said elevations are based upon positional solutions derived from RTK GNSS observations using the Ohio Department of Transportation's Ohio Real Time Network equipment and software and the National Geospatial Survey's GSD18 model. Elevations were then transferred to conventional leveling procedures to the permanent benchmarks listed herein. (Fieldwork Completed 12/2023).

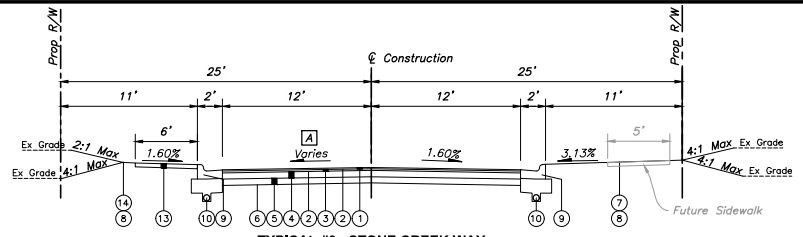
Bench Mark	Elevation	Description
BM#1	959.94	Chaised "X" on southwest flange bolt of a fire hydrant, located on the northeasterly side of West Cherry Street (State Route 37) being the first fire hydrant (225 feet) northwest of the intersection with Kintner Parkway.
BM#3	953.78	Chaised "X" on northeast rim of a sanitary sewer manhole, located 150 feet west of the southwest bulling corner of 700 West Cherry Street (State Route 37). Being on the southwest side of West Cherry Street.

Points	Northing (Ground)	Eastng (Ground)	Elevation	Full Description
6	212822.0140	186449.9070	962.20	1185 #6 MAGS
204	212937.7260	1863838.7650	954.13	1182 RTK IP#w/cap
205	212942.2620	1864391.2620	957.80	1182 RTK IP#w/cap
817	213653.4350	1863179.5940	952.50	1056 #17 CISO MAGS
520	213448.4670	1863752.1780	959.90	1057 #20 CISO RP#w/cap
522	213601.0310	1863996.8320	962.31	1057 #22 CISO RP#w/cap

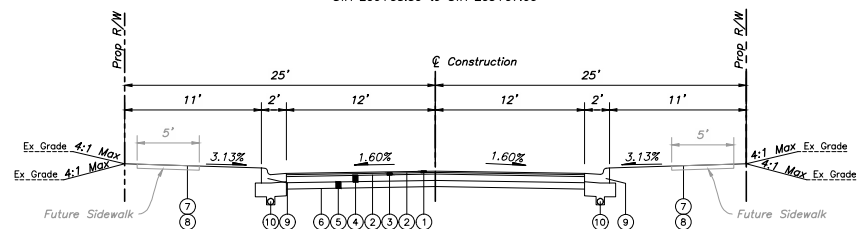
- 1 LARRY M. ("MIKE") FORMAN
LUANN FORMAN
MARY LO FORMAN MILLER,
SUCCESSOR CO-TRUSTEES
O.451 AC. (DEED)
O.R. 1088, P. 2928
- 2 PURELIFE PROPERTIES LLC
P.N. 41713402004000
- 3 RKJ JUDY LLC
P.N. 41713402005006
- 4 WOLFE, CHARLES E
P.N. 41713404007000
- 5 FRANK ROBERT A &
JANET M CO TRUSTEES
P.N. 41713404006000
- 6 OBERLINDER STEVEN D &
BRENDA
P.N. 41713404006000
- 7 ENDRES JOHN ARTHUR
P.N. 41713404004000
- 8 SON HONG DUC TRU MY
& NGUYEN HOAI SON
P.N. 41713404003000
- 9 LEFEVER TAMI LYNN
BRENDA
P.N. 41713408002000
- 10 FRIBONIC MICHAEL
P.N. 41713404001000
- 11 LIMING JASON L TRUSTEE
P.N. 417134040016000
- 12 REYNOLDS MARK HENRY
P.N. 41713408004000

- A **© CURVE DATA**
P.I.=Sta 201+03.01
Δ=1°26'46"
Dc=5'43'46"
R=1000.00'
T=12.63'
L=25.25'
E=0.08'
C=25.25'
CB=547'44'46"E
- B **© CURVE DATA**
P.I.=Sta 207+41.76
Δ=2°14'12"
Dc=32'44'26"
R=175.00'
T=33.68'
L=133.26'
E=3.21'
C=66.14'
CB=536'07'46"E
- C **© CURVE DATA**
P.I.=Sta 208+85.41
Δ=6°10'45.3"
Dc=45°50'12"
R=125.00'
T=73.75'
L=133.26'
E=20.13'
C=127.04'
CB=555'46'37"E
- D **© CURVE DATA**
P.I.=Sta 213+23.04
Δ=5°10'30"
Dc=48'33'21"
R=118.00'
T=56.35'
L=105.14'
E=12.76'
C=101.69'
CB=N68°09'27"E
- E **© CURVE DATA**
P.I.=Sta 104+97.38
Δ=50°45'59"
Dc=10°08'51"
R=564.94'
T=268.05'
L=500.56'
E=60.37'
C=484.35'
CB=N68°16'31"E

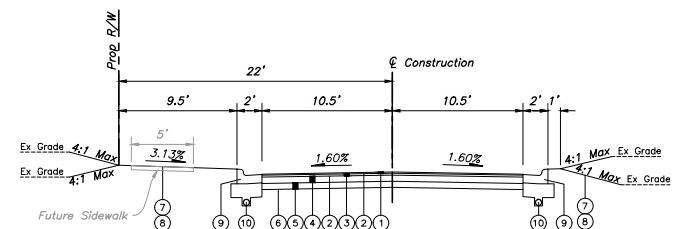
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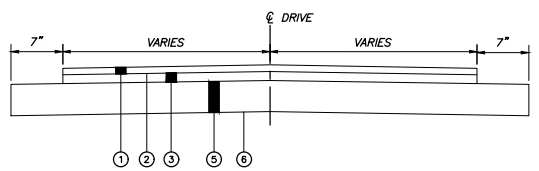
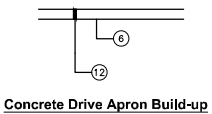
TYPICAL #3 - STONE CREEK WAY
APPLIES:
STA 200+12.00 to STA 200+33.50 - See Intersection Details Sheet 39
STA 200+33.50 to STA 203+97.00



TYPICAL #4 - STONE CREEK WAY
APPLIES:
STA 203+97.00 to STA 207+63.13
STA 207+63.13 to STA 208+56.55 - See Intersection Details Sheet 35



TYPICAL #5 - STONE CREEK WAY
APPLIES:
STA 208+56.55 to STA 210+44.25
STA 210+44.25 to STA 211+10.25 - See Drive Details Sheet 42
STA 211+10.25 to STA 212+35.11
STA 212+35.11 to STA 213+63.36 - See Intersection Details Sheet 40



LEGEND

- ① ITEM 441 - 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- ② ITEM 407 - NON-TRACKING TACK COAT (@ 0.055 Gal/Sq Yd)
- ③ ITEM 441 - 1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448), PG64-22
- ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22
- ⑤ ITEM 304 - 6" AGGREGATE BASE
- ⑥ ITEM 204 - SUBGRADE COMPACTION AND PROOF ROLLING
- ⑦ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑧ ITEM 653 - TOPSOIL T=3"
- ⑨ ITEM 609 - COMBINATION CURB AND GUTTER (DCED-R2020)
- ⑩ ITEM 605 - 4" PIPE UNDERDRAIN
- ⑪ ITEM 609 - 9" CONCRETE MEDIAN (COLS STND DWG 2331)
- ⑫ ITEM 452 - NON-REINFORCED CONCRETE T=8"
- ⑬ ITEM 608 - CONCRETE WALK (T=4")
- ⑭ ITEM 659 - NATIVE WILDFLOWER AND GRASS MIXTURE

A = Cross slope shall be 1.60% except as as shown on Intersection Details Sheet 38

C:\Users\jaf\OneDrive\Documents\2025\Cheshire\2025\Cheshire_Road_Relocation.dwg, User: jaf, Date: 09/22/2025, 1:52 PM, User: jaf, Plot Date: 09/22/2025, 1:52 PM, Plot Size: 11.00 x 17.00

CITY OF SUNBURY GENERAL NOTES

1. A PRE-CONSTRUCTION MEETING SHALL BE HELD AT THE CITY ADMINISTRATION BUILDING (9 E GRANVILLE ST) AT LEAST 15 CALENDAR DAYS BEFORE THE START OF CONSTRUCTION. REPRESENTATIVES OF THE OWNER, DESIGN ENGINEER, DELAWARE COUNTY ENGINEER'S OFFICE, AND THE CONTRACTOR SHALL BE IN ATTENDANCE. A SCHEDULE OF THE SEQUENCE OF EVENTS DURING CONSTRUCTION MUST BE SUBMITTED FOR REVIEW AT LEAST (7) SEVEN DAYS PRIOR TO THIS MEETING.
2. THE CITY OF SUNBURY DETAILED SPECIFICATIONS TOGETHER WITH THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS (CMS), INCLUDING ALL SUPPLEMENTS THERETO, CURRENT EDITION, ALONG WITH THE DELAWARE COUNTY DESIGN, CONSTRUCTION AND SURVEYING STANDARDS, LATEST EDITION & ODOT 2023 EDITION SHALL GOVERN ALL MATERIAL AND WORKMANSHIP INVOLVED IN THE IMPROVEMENTS SHOWN IN THE PLANS UNLESS OTHERWISE NOTED.
3. ALL WORK SHALL BE COMPLETELY ACCEPTABLE TO THE CITY OF SUNBURY OFFICIALS. NO WORK SHALL COMMENCE UNTIL ARRANGEMENTS HAVE BEEN MADE WITH THE CITY OF SUNBURY ENGINEER FOR INSPECTION. NECESSARY LINE AND GRADE STAKING TO BE PROVIDED BY THE CONTRACTOR.
4. THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE CITY OF SUNBURY ENGINEER AT LEAST (7) SEVEN WORKING DAYS PRIOR TO THE START OF CONSTRUCTION.
5. APPROVED WORKING HOURS: 7A-7P MON-FRI EXCEPT ON NATIONAL HOLIDAYS. CONTRACTOR MAY REQUEST SATURDAY WORKING HOURS AT THE PRECON MEETING.
6. THE CONTRACTOR SHALL PROVIDE A VIDEO RECORD OF THE SITE TO THE CITY ENGINEER PRIOR TO THE START OF CONSTRUCTION. EMPHASIS OF THE VIDEO SHOULD BE ON EXISTING DEVELOPED AREAS CONTIGUOUS WITH THE PROJECT WORK AREAS.
7. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AN UP-TO-DATE COPY OF THE APPROVED PLANS. FOR ACCEPTANCE OF WORK, THE CITY REQUIRES A SIGNED COMPLETE SET OF ALL APPROVED PLANS.
8. THE CONTRACTOR SHALL SECURE AND PAY FOR ALL PERMITS AND GOVERNMENT FEES, LICENSES AND INSPECTIONS FOR THE PROPER EXECUTION OF THE IMPROVEMENTS SHOWN ON THE PLANS.
9. ALL PERTINENT STANDARD CONSTRUCTION DRAWINGS ARE AVAILABLE UPON REQUEST AT THE OFFICE OF THE CITY ENGINEER.
10. APPROVAL OF THESE PLANS SHALL BE IN ACCORDANCE WITH THE CITY OF SUNBURY.
11. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VISIT THE SITE AND VERIFY THE EXTENT OF WORK TO BE PERFORMED PRIOR TO MAKING THEIR BID. THIS IS ESPECIALLY TRUE WITH REGARD TO ANY REMOVAL ITEMS.
12. THE CONTRACTOR AND SUBCONTRACTOR SHALL ALSO ABIDE BY ALL ORDINANCES OF THE CITY OF SUNBURY.
13. THE CONTRACTOR IS RESPONSIBLE FOR THE INVESTIGATION, LOCATION, SUPPORT, PROTECTION, AND RESTORATION OF ANY EXISTING UTILITIES AND APPURTENANCES WHETHER SHOWN ON THESE PLANS OR NOT. THE CONTRACTOR SHALL EXPOSE ALL UTILITIES OR STRUCTURES PRIOR TO CONSTRUCTION TO VERIFY THE VERTICAL AND HORIZONTAL EFFECT ON PROPOSED CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO WORK IN THE VICINITY OF THEIR UNDERGROUND LINES. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE RELOCATION OF ANY UTILITIES AS REQUIRED BY THE PLAN WITH THE OWNER OF THE AFFECTED UTILITY.
14. ALL FIELD TILE BROKEN DURING EXCAVATION SHALL BE REPLACED TO ORIGINAL CONDITION OR CONNECTED TO THE CURB SUBDRAIN OR TO THE STORM SEWER SYSTEM AS DIRECTED BY THE ENGINEER.
15. INGRESS AND EGRESS SHALL BE MAINTAINED TO PUBLIC AND PRIVATE PROPERTY.
16. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY AND ALL EXISTING WORK DAMAGED DURING OR DUE TO THE EXECUTION OF THIS CONTRACT. AT THEIR OWN EXPENSE, SAID WORK TO BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER'S ENGINEER AND THE CITY OF SUNBURY.
17. ALL SIGNS, FENCES, SHRUBS, DRAINAGE STRUCTURES, OR OTHER PHYSICAL FEATURES THAT ARE TO REMAIN INTACT WHICH ARE DISTURBED OR DAMAGED DURING WORK UNDER THE CONTRACT SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR UNLESS OTHERWISE PROVIDED IN THE CONTRACT. THE COST OF ALL SUCH WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS SANITARY SEWER ITEMS.
18. THE CONTRACTOR SHALL RESTORE DISTURBED AREAS TO ORIGINAL CONDITION AS NEAR AS PRACTICABLE UPON COMPLETION OF THIS WORK. ANY DAMAGE TO OTHER UTILITIES DURING THIS WORK BY THE SUBCONTRACTOR SHALL BE REPAIRED BY THE APPROPRIATE UTILITY OWNER AT THE CONTRACTOR'S EXPENSE.
19. CARE SHALL BE EXERCISED WHEN WORKING THE AREA AROUND EXISTING TREES AND SHRUBS. TREES MAY BE REMOVED WITHIN THE PERMANENT AND TEMPORARY EASEMENTS ONLY. EFFORTS SHALL BE MADE TO PRESERVE TREES 24-INCHES CALIPER OR LARGER. NO TREES SHALL BE DAMAGED OR DISTURBED OUTSIDE OF THE EASEMENTS OR WHERE MARKED ON THE PLANS AS "DO NOT DISTURB".

20. THE COST OF DEWATERING OR ROCK EXCAVATION SHALL BE INCLUDED IN THE PRICE BID FOR SANITARY PIPE. THE BIDDER SHALL DETERMINE IF ANY DEWATERING OR ROCK EXCAVATION WILL BE REQUIRED AND ADJUST THEIR BIDS ACCORDINGLY.
21. PAVEMENT CUTS FOR UTILITY LINE INSTALLATIONS SHALL BE SUBJECT TO THE BACKFILL REQUIREMENTS OF COC CMS ITEM 912. PAVEMENT SHALL BE PLACED TO MATCH EXISTING SECTION OR 10 INCHES OF 448 ASPHALT, WHICHEVER IS GREATER. DEEP TRENCHES MAY NOT BE PARTIALLY FILLED WITH LARGE SIZE AGGREGATE.
22. THE FLOW IN ALL SEWERS, DRAINS, AND SANITARY SEWER COURSES ENCOUNTERED SHALL BE MAINTAINED BY THE CONTRACTOR AT THEIR OWN EXPENSE AND WHENEVER SUCH SANITARY SEWER COURSES AND DRAINS ARE DISTURBED OR DESTROYED DURING THE PROSECUTION OF EXPENSE TO THE CITY OF SUNBURY.
23. ALL EARTHWORK OPERATIONS, ESPECIALLY PAVEMENT SUBGRADE CONSTRUCTION, SHALL BE INSPECTED BY A REGISTERED SOILS ENGINEER EMPLOYED AND PAID FOR BY THE OWNER. ADDITIONALLY ALL FINAL GRADES SHALL BE FIELD CHECKED BY THE CONSTRUCTION MANAGER UPON COMPLETION OF THE CONTRACTOR'S OPERATIONS TO DETERMINE IF THE SITE HAS BEEN CONSTRUCTED TO THE GRADES INDICATED.
24. ALL ITEMS CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR AND THE COST OF SAME AND SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS.
25. EROSION CONTROL MEASURES ARE TO BE INSTALLED PER PLAN OR AS SUCH DIRECTED BY THE CITY AND ARE TO BE MAINTAINED UNTIL SUCH TIME THAT THEY ARE NO LONGER REQUIRED.
26. ALL AREAS IN THE RIGHT-OF-WAY ARE TO BE GRADED AND SEEDED AS SOON AS WORK IN THAT AREA IS COMPLETE.
27. THE CONTRACTOR IS RESPONSIBLE FOR THE PROVISION AND MAINTENANCE OF A PORTABLE TOILET ON THE SITE DURING ALL PHASES OF CONSTRUCTION.
28. THE CONTRACTOR AND/OR DEVELOPER IS RESPONSIBLE FOR ALL OPERATIONS AS WELL AS ASBESTOS AND CONTAMINATE TESTING. TESTING IS TO BE PERFORMED BY A REGISTERED CONTRACTOR AS SPECIFIED BY THE CITY ENGINEER.
29. THE CITY ENGINEER RESERVES THE RIGHT TO DIRECT PROJECT SPECIFIC CHANGES TO ANY CITY CONSTRUCTION NOTE AND SPECIFICATION.
30. NO CONSTRUCTION TRAFFIC IS PERMITTED ON STREETS ACCESSING NEIGHBORHOOD SUBDIVISIONS. NO TRACKED EQUIPMENT IS PERMITTED ON EXISTING STREET PAVEMENT.
31. THE CONTRACTOR AND SUBCONTRACTORS SHALL BE RESPONSIBLE FOR COMPLYING WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970 AND ALL OTHER FEDERAL, STATE, AND LOCAL SAFETY REQUIREMENTS, TOGETHER WITH EXERCISING PRECAUTIONS AT ALL TIMES FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. THE CONTRACTOR AND SUBCONTRACTORS SHALL INITIATE, MAINTAIN, AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS, AND PROGRAMS ASSOCIATED WITH THIS WORK.
32. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL PERMITS NECESSARY TO PERFORM ALL WORK WITHIN AND/OR ADJACENT TO THE RIGHT-OF-WAY OF ALL ROADS AND HIGHWAYS. ALL FEES, BONDS, INSURANCE, AND OTHER COSTS REQUIRED BY THE OWNER OF SAID ROADS AND HIGHWAYS SHALL BE PROVIDED BY THE CONTRACTOR AND INCLUDED IN THE PRICE BID FOR THE WORK.
33. THE CONTRACTOR SHALL PROVIDE THE CITY ENGINEER AND DELAWARE COUNTY ENGINEER WITH A 24 HOUR TELEPHONE NUMBER TO READILY CONTACT A RESPONSIBLE PARTY IN THE CASE OF AN EMERGENCY. THE COST AND/OR DAMAGES INCURRED RELATED TO WORK PERFORMED BY THE CONTRACTOR IN SUCH EMERGENCIES ARE THE CONTRACTORS RESPONSIBILITY AND NOT THE AFFECTED ENTITY.
34. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR BY REASON OF COMPLIANCE WITH ANY OF THE REQUIREMENTS INDICATED IN THE PLANS, BUT PAYMENT SHALL BE DEEMED TO BE INCLUDED AMONG THE SEVERAL ITEMS, AS BID UPON, UNLESS OTHERWISE SPECIFICALLY PROVIDED.
35. IN ADDITION TO THE DIRECT REQUIREMENTS OF THE CONTRACT SPECIFICATIONS, THE CONTRACTOR SHALL OBSERVE AND CONFORM TO THE SPECIFIC REQUIREMENTS OF ALL RIGHT-OF-WAYS INCLUDING EASEMENTS, COURT ENTRIES, RIGHTS OF ENTRY, OR ACTION FILED IN COURT IN ACCORDANCE WITH THE CODE OF THE APPLICABLE GOVERNING AGENCY. THE COST OF OPERATIONS NECESSARY TO FULFILL SUCH REQUIREMENTS SHALL BE INCLUDED IN THE TOTAL CONTRACT PRICE BID.
36. THE DESIGN SHOWN ON THESE DRAWINGS IS BASED UPON A FIELD SURVEY AND EXISTING RECORDS.
37. PRIOR TO THE BEGINNING OF CONSTRUCTION, THE CONTRACTOR SHALL TAKE PHOTOGRAPHS AND VIDEO OF THE PROJECT SITE PER THE SPECIFICATIONS. A DIGITAL COPY OF ALL MEDIA SHALL BE SUBMITTED TO THE CITY ENGINEER FOR REVIEW AND APPROVAL. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR PRE-CONSTRUCTION VIDEO/TAPING.

38. ALL CONSTRUCTION STAKING SHALL BE PROVIDED BY THE CONTRACTOR. THIS WILL CONSIST OF ALIGNMENT AND GRADE STAKES SET AND MARKED IN THE FIELD. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY REPLACEMENT STAKES NECESSARY, REMOVED OR DESTROYED BY THEIR PERSONNEL OR EQUIPMENT.
39. THE CONTRACTOR SHALL MAINTAIN AS-BUILT DRAWINGS THROUGHOUT THE PROJECT AND SHALL BRING THE CURRENT SET OF AS-BUILT DRAWINGS TO EACH PROGRESS MEETING FOR REVIEW.
40. THE WORK OF THIS CONTRACT SHALL NOT INTERFERE WITH MAINTAINING CONTINUOUS FLOW IN THE EXISTING SANITARY SEWER, STORM SEWER, WATERLINES, POWER, TELEPHONE, CABLE TV, GAS, OR ANY OTHER SYSTEM THAT MAY BE ENCOUNTERED DURING CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN ALL UTILITY POLES AND RELOCATE ANY UTILITY THAT IMPEDES CONSTRUCTION. NOTHING SHALL BE DONE BY ANY OTHER CONTRACTOR FOR THIS PROJECT WHICH WILL IN ANY WAY REDUCE THE QUALITY OR QUANTITY LEVEL OF SUCH OPERATIONS. THE CONTRACTOR SHALL NOTIFY THE OWNER (1) ONE WEEK PRIOR TO MODIFYING ANY FACILITIES. NO PAYMENT WILL BE MADE FOR ANY UTILITY MAINTENANCE UNLESS SPECIFICALLY NOTED AS A BID ITEM IN THE PROPOSAL. FAILURE TO PROPERLY MAINTAIN UTILITIES WILL RESULT IN THE REDUCTION OF THE PROPOSAL ITEM FOR UTILITY MAINTENANCE.
41. AT ALL UTILITY CROSSINGS, THE BACKFILL SHALL CONSIST OF COMPACTED GRANULAR MATERIAL BETWEEN THE DEEPER AND SHALLOWER PIPE UNLESS CONTROLLED DENSITY FILL OR CONCRETE IS SPECIFIED OTHERWISE IN THE DRAWINGS.
42. MANHOLES SHALL BE INSTALLED WITH ADJUSTING RINGS SO THAT THE TOP OF THE CASTING IS AT EXISTING GRADE. THE CASTING SHALL NOT PROTRUDE SO AS TO INTERFERE WITH MOWING.
43. BACKFILL: EARTH WILL NOT BE MOUNDING OVER THE TRENCH IN ANTICIPATION OF SETTLEMENT. THE CONTRACTOR SHALL COMPACT THE TRENCH AND ESTABLISH THE ORIGINAL DITCH GRADE AS WORK PROGRESSES. DRAINAGE MUST BE RESTORED AS QUICKLY AS PRACTICAL. IF EROSION OF THE DITCH LINE CAUSES SILTING OF EXISTING DRAINAGE LINES, THE CONTRACTOR SHALL CLEAN AS NECESSARY. BACKFILL AND CLEANUP OPERATIONS SHALL BE KEPT WITHIN 300 FEET OF PIPE LAYING OPERATIONS DAILY. CARE SHALL BE TAKEN TO INSURE THAT NO DITCH LINE ELEVATIONS ARE RAISED DUE TO EXCESS BACKFILL. COMPACTION IS TO BE CARRIED OUT WITH THE NECESSARY EQUIPMENT TO ACHIEVE THE COMPACTION REQUIREMENT OF ODOT CMS ITEM 304.05. THE CONTRACTOR SHALL MAINTAIN DRIVEWAYS ACCESSIBLE AT ALL TIMES BY USING PLATES OR COMPACTED GRANULAR FILL.
44. ROAD MAINTENANCE AREA: UNLESS PREEXISTING BEFORE CONSTRUCTION OPERATIONS, THE SURFACE WITHIN A MINIMUM OF 5 FEET FROM THE EDGE OF PAVEMENT SHALL BE MAINTAINED OBSTACLE FREE, BY THE CONTRACTOR FOR POSITIVE DRAINAGE AND/OR SNOW FLOW/MAINTENANCE OF ROAD PAVEMENT.
45. ROOF DRAINS, FOUNDATION DRAINS, AND OTHER CLEAN WATER CONNECTIONS TO THE SANITARY SEWER SYSTEM ARE PROHIBITED.
46. DISTURBANCE TO THIS SITE IS COVERED BY OEPA GENERAL PERMIT 0HC00006 AND THE NPDES PERMIT.
47. MARKER POLES SHALL BE SET AT ALL PROPOSED MANHOLES AS SHOWN ON THE PLANS. THE MARKER POLE SHALL BE A 4 INCH X 4 INCH PRESSURE TREATED OR HARDWOOD POLE SET TO A POINT 3 FEET BELOW FINAL GRADE. BRACE THE POLE IN SUCH A MANNER TO HOLD IT FIRMLY IN POSITION DURING BACKFILL. THE POLE SHALL EXTEND 4 FEET ABOVE FINAL GRADE AND SHALL BE PAINTED BRIGHT RED. THE COST FOR THE MARKER POLES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 604-MANHOLES.

ROADWAY CONSTRUCTION SPECIFICATIONS:

1. CITY STREETS, AS WELL AS STATE ROUTES, ARE TO BE KEPT CLEAN AND FREE. AT ALL TIMES, FROM MUD, STONE, DIRT, ETC. A TEMPORARY CONSTRUCTION ENTRANCE COMPRISED OF 100'X20' MAT OF 2 INCH STONE, IS TO BE MAINTAINED AT ALL TIMES.
2. ASPHALT JOB MIX FORMULA WILL BE REQUIRED AS WELL AS OTHER TESTING AS DESCRIBED IN ITEM 401.
3. PLACEMENT OF THE SURFACE COURSE OF 448 ASPHALT MAY BE DELAYED UNTIL THE MAJORITY OF CONSTRUCTION TRAFFIC HAS DIMINISHED AT THE OPTION OF THE DEVELOPER.
4. CONCRETE CURBS ARE TO BE BRANDED DURING CONSTRUCTION AS FOLLOWS:
 "S" - ON TOP OF CURB FOR SANITARY LATERAL LOCATIONS
 "W" - ON FACE OF CURB FOR WATER SERVICE BOX LOCATIONS
 "VW" - ON FACE OF CURB FOR HYDRANT WATCH VALVE LOCATIONS
 "WM" - ON FACE OF CURB FOR MAIN LINE VALVE LOCATIONS
 BRANDS THAT ARE MISSED MUST BE NEATLY GROUND IN THE CURB AFTER IT HAS SET.
5. ALL ROADWAY SUBGRADE SHALL BE PROOF ROLLED PER ITEM 204 AND HAVE A REGISTERED SOILS ENGINEER ON SITE TO WITNESS AND REPORT ON THE PROOF ROLLING.

LIGHTING:
SEE SHEET 33 FOR LIGHTING NOTES.
 MAINTENANCE OF TRAFFIC:
SEE SHEET 13 FOR MAINTENANCE OF TRAFFIC NOTES
 TRAFFIC SIGNAL:
SEE SHEET 51-53 FOR TRAFFIC SIGNAL NOTES

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GENERAL NOTES

CHESHIRE ROAD RELOCATION

DELCO WATER COMPANY
GENERAL NOTES FOR WATERLINE CONSTRUCTION
 REVISED: JANUARY 16, 2025

1. General Notes as modified by Del-Co Water and shown on the approved Construction Drawings shall supersede the requirements of the Del-Co Water Company Construction Standards Manual wherever discrepancies occur.

2. Standard General Notes:

a. Waterline design, materials, and installation methods shall conform to applicable sections of Recommended Standards for Water Works (Ten States Standards), American Water Works Association (AWWA) Standards, and the Del-Co Water Company Construction Standards Manual. Contractor shall obtain a copy of the standards and have in their possession at all times during construction. Coordinate work with Del-Co Water (740) 548-7746.

b. Del-Co Water's signature on this plan signifies only concurrence with the general purpose and location of the proposed waterline improvements. All technical details remain the responsibility of the professional engineer who prepared and certified these plans. Del-Co Water Company takes no responsibility, financial or otherwise, regarding errors in this plan.

c. Correction of errors shall be to the applicable Del-Co Water Company standard, and the sole responsibility of the developer. All corrections, or revisions that affect Del-Co's Waterline plans, directly or indirectly shall be submitted to, and approved by Del-Co Water Company prior to revisions being issued.

d. The contractor is responsible for the submission of GPS coordinates to the Del-Co Water GIS Department at the completion of the waterline installation. Based on the circumstances of the project, points may be required to be uploaded daily. These coordinates shall include all horizontal and vertical (Northing, Easting, Elevation) survey coordinates. The survey coordinates shall be obtained at the completion of water main installation and shall include all valves, tees, fire hydrants, bends, plugs, reducers, tapped tees, curb boxes, air releases, 2" end of line fire hydrants, ends of casing pipe, and service saddles. GPS points and depths are also required on service line bores at each side of the road in addition to the saddle/water main and meter locations. GPS points should also be collected every 10 feet on services lines that are being deflected. Additional GPS coordinates are required on top of the exposed water main every 60' on straight runs, and at every bell (20') where the waterline is being deflected without fittings and where the trench can be safely entered. All GPS points shall be collected on the center or centerline of the feature. For hydrants, post indicator valves and 2" flush hydrants, the point correct location. To ensure accuracy, the GPS rod and level must be utilized for all points. All GPS points shall also have pertinent attributes collected, especially pipe material and pipe diameter where applicable. If the coordinates are unable to be collected on top of the water main or fitting, it shall be notated in the GPS attributes. If a coordinate is unable to be collected directly on top of the feature, an offset point must be collected by a capable survey grade unit.

i. GPS coordinates shall be referenced to the applicable county engineer's monuments and shall be based on the North American Datum of 1983 (NAD with the NSRS2007 adjustment, with further reference made to the Ohio State Plane North Coordinate System, North Zone, with elevations based on NAVD 88 Datum. All coordinates (Northing, Easting, Elevation) shall be referenced to the nearest hundredth. All survey coordinates shall be accurate to within 0.6 foot or less horizontal and vertical.

ii. The GPS coordinates shall be submitted to the Del-Co Water GIS Department in digital form, excluding Portable Document Format (PDF). Preferred file types include comma delimited text files and Microsoft Excel. The above-listed GPS coordinate information shall be submitted to the Del-Co Water GIS or Engineering Department as part of the as-built drawing submittal where applicable.

iii. Contractors are required to supply (own or rent) and maintain their own GPS devices. Del-Co Water reserves the right to deny inadequate GPS devices that are unable to provide sufficient accuracy required for submission.

e. As-Built drawings are required following the completion of construction. One set of drawings marked "As-Built" shall be submitted by the developer to Del-Co's Inspection Department for review and approval. Water mains cannot receive a Final Acceptance until as-built drawings have been approved. Please note: taps may not be purchased nor installed until water lines have received a Final Acceptance

f. Water mains shall become the ownership of Del-Co Water upon final acceptance.

g. Waterline construction plans are approved for a period of one year from the date of the approval letter or signed plans. If construction has not started within one year of the date of approval, plans shall be resubmitted to Del-Co Water Company for approval.

3. Waterline Construction:

a. Waterlines shall be NSF 61 approved, and compliant with ASTM D2241 & Ohio EPA ENG-08-002 standards.

b. Use the following type and class of pipe unless otherwise indicated on the drawings:

i. 2-inch water line pipe: Class 200 SDR 21 Yelamine PVC (restrained joint).

ii. 4-inch water line pipe: Class 200 SDR 21 PVC.

iii. 6-inch water line pipe: Class 200 SDR 21 PVC.

iv. 8-inch to 12-inch water line pipe: Class 160 SDR 26 PVC.

v. 16-inch and larger water line pipe: AWWA C151 Class 52 DIP.

vi. 4-inch pipe and larger used for fire service: AWWA C900 DR 18 (150 PSI) PVC.

vii. All sizes of Del-Co-owned waterlines used on master meter projects: Class 200 SDR 21 PVC.

c. All sizes of Ductile Iron pipe shall be installed with V-Bio Enhanced Polyethylene Encasement and in accordance with Del-Co Water Subdivision Construction Standards Section 02731 Ductile Iron.

d. All fittings shall be mechanical joint conforming to AWWA C153.

e. Mechanical Joint or Transition Accessory Pack T-bolts shall be corrosion resistant.

f. Crosses shall not be used without approval of Del-Co Water Company.

g. All valves shall be mechanical joint conforming to AWWA with AISI 304 stainless steel external hardware and shall be Resilient-Seated Gate valves per AWWA C509. Use of butterfly valves is prohibited.

h. Provide heavy-duty valve boxes on all hot-taps and at valves located under gravel or pavement surfaces.

i. Top of valve box shall be flush with finished grade in paved areas, and 4 inches above finished grade in non-paved areas.

j. Maintain a minimum 10-foot horizontal and 1.5-foot vertical separation between waterlines and sanitary and storm sewers.

k. All other buried utilities shall maintain a minimum 5-foot horizontal separation, and 2-foot vertical separation from the centerline of waterlines as finally laid and constructed.

l. Provide concrete thrust blocking for all fittings, valves, anchor tees, and hydrants. m. Bury waterlines a minimum depth of 48-inches to the top of pipe.

n. All engineered fill to be placed over or under Del-Co Water Lines shall be in place prior to the construction of the water lines.

o. Place a 5-foot steel fence post or 4"x4" wood post at valves and the ends of lines. Paint blue.

p. Tracer Wire:

i. Install Copperhead® or equal 12-gauge high strength 452lb break strength 30 mil HDPE jacket, copper-clad, steel reinforced tracer wire on all water mains and service lines installed by trenching methods.

ii. Install Copperhead® or equal 12-gauge extra high strength 1150lb break strength 45 mil HDPE jacket, copper-clad, steel reinforced tracer wire on all water mains and service lines installed by boring methods.

iii. Fasten wire to pipe in two places per pipe section. Extend tracer wire to ground surface at all valves and hydrants as shown in the Del-Co standard detail. Splice wires using Bundy Copper Split Bolt KS-15. Thoroughly wrap the connector and bare wires with 3M Temflex 2155 Rubber Splicing Tape, cover entire connection with Scotch Super 88 Heavy Duty Grade Electrical Tape.

iv. Connect all service line wires to main line wires using Bundy Copper Split Bolt KS-15. Thoroughly wrap the connector and bare wires with 3M Temflex 2155 Rubber Splicing Tape, cover entire connection with Scotch Super 88 Heavy Duty Grade Electrical Tape.

v. Contractor Shall Test the continuity of all wire using a third-party tester.

1. Contractor shall contact Del-Co's Inspection Department a minimum of 24 hours prior to test.

2. Tester shall send a signed report to Del-Co Water Inspection Department affirming all wire has continuity.

3. Contractor shall repair all deficiencies.

4. Water Service Construction (between main line and meter pit):

a. All water services shall be constructed and installed per AWWA C800.

b. For meters 1-inch and smaller, use 1-inch Class 200 SDR 7 iron pipe size (IPS) polyethylene pipe.

c. Connections to PVC pipe shall be made with approved tapping saddle and corporation stop per Del-Co Subdivision Construction Standards Manual.

d. Connections to DIP shall be by direct tap or saddle and approved corporation stop per Del-Co Subdivision Construction Standards Manual.

e. Provide a curb stop with 1-inch female iron pipe threads on the customer side at the end of service lines. Locate at ROW, but a minimum of five feet from edge of sidewalk unless otherwise approved or directed by Del-Co Water.

f. All service line valves 1½-inch and larger shall be mechanical joint gate valves, restrained with Duct-Lugs and galvanized all thread rod, or anchor tees where applicable.

g. Minimum depth of cover shall be 48 inches.

h. Place a 5-foot steel fence post or 4"x4" wood post at the ends of all service lines, paint blue.

i. Contractor shall be responsible for providing special backfill material for all lines, including those installed by Del-Co Water Company, where required by the County or City Engineer.

5. Fire Hydrants:

a. Fire hydrants shall conform to AWWA C502 for dry barrel hydrants.

b. Main valve: 5.25-inch compression.

c. Nozzles:

i. Threading: Conform to NFPA National Standard fire hose threads.

ii. 4.5-inch steamer, except in the following locations provide an integral storz connection: Harrington HHS50 or equal; Berkshire, Berlin, Concord, Genoa, Kingston, Liberty, Orange, Porter, Trenton, and Scioto Townships; City of Powell; and the Villages of Sunbury and Galena.

iii. Two 2.5-inch hose connections.

d. Inlet Connection: 6-inch mechanical joint.

e. Operating Nut: 1.5-inch pentagon, turn counterclockwise to open.

f. Placement of Hydrant: 2 feet from the back of curb or 8 feet from the edge of pavement on non-curbed streets

g. Extensions and parts: Shall be manufactured by the original equipment manufacturer.

h. Approved Manufacturers: Mueller Super Centurion 200, American Flow Control B-84-B, Clow Medallion, M&H Model 129M, AVK Nostalgic 2780, or Kennedy Guardian K81D.

i. Painting: Repaint all hydrants after installation per Del-Co Subdivision Construction Standards Manual Section 02731, Part 2.06.

i. Liberty Township, Powell, and Village of Sunbury: Safety Red.

ii. Other Townships: Safety Yellow

iii. Fire hydrants located on private water lines shall be painted Industrial Green.

6. Flushing Hydrants

a. All dead ends line shall have either an automatic flushing device or a 2" end of line fire hydrant in accordance with Section 02731 Flushing Hydrants.

7. Disinfect all water lines in accordance with AWWA C651 and Del-Co Water specifications.

8. All pipe installation and pressure testing shall be in accordance with AWWA C600 for ductile iron pipe and C605 for plastic pipe, and Del-Co Specifications.

a. Contractor shall provide all equipment necessary to perform pressure test.

b. Schedule test between 8:00 AM and 2:00 PM weekdays. Notify Del-Co Water 24 hours prior to testing.

9. Obtain written approval of material and manufacturers list from Del-Co Water prior to beginning construction.

10. Provide casing pipe for all road crossings unless otherwise approved by Del-Co. Casing pipe shall be steel pipe with 0.375-inch wall thickness, or PVC C900 for waterlines 12-inch Dia. or less. Casings for waterlines larger than 12-inch Dia. may be AWWA C905.

11. Easements shall be provided to Del-Co Water before permission will be given to make new service line connections.

12. Connections to existing water lines will be made by Del-Co Water at the contractor's expense or performed by contractors who are approved for making connections. Notify all property owners, and Del-Co Water, in writing 48 hours before starting construction.

13. Contractor shall excavate to determine the location and depth of existing water lines wherever cover over the water lines is being reduced. If the final depth of the waterline will be below Del-Co Water standards, Contractor shall submit a relocation plan for approval by Del-Co Water, and relocate the water line at their expense.

14. Contractor shall be responsible for locating and protecting the water line prior to final acceptance by Del-Co Water, and repairing all damages from construction activity.

15. Do not fill new water lines until approved by Del-Co Water.

16. Booster pumps are not allowed on individual services.

17. All water mains, including those not designed to provide fire protection, shall be sized following an analysis based on flow demands, and pressure requirements. The system shall be designed to maintain a minimum pressure of 20 psi at ground level at all points in the distribution system under all designed flow conditions. Normal working pressure in the distribution system shall not be less than 35 psi.

18. Del-Co Water requires all waterlines intended for ownership and maintenance by Del-Co Water, regardless of pipe size, to secure EPA approval prior to the start of construction.

CALCULATED
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GENERAL NOTES

CHESHIRE ROAD RELOCATION

6
 65

ROUNDING

The rounding at slope breakpoints shown on the Typical Sections applies to all Cross Sections even though otherwise shown.

SUBGRADE COMPACTION AND PROOF ROLLING

Construct the subgrade as follows and in the following sequence.

1. After clearing and grubbing, strip topsoil within the limits of grading.
2. Following site stripping, excavate the subgrade to within 0.2 of a foot of the plan subgrade elevation.
3. Remove any remaining vegetation, topsoil, organic material, existing asphalt and granular base, and any other unsuitable materials (e.g., A-4B, A-2-5, A-5, A-7-5, coal, shale, rock) as determined by the Engineer from the proposed pavement areas. These materials are to be removed prior to proof rolling. Areas of over excavation for removed unsuitable materials shall be filled in a controlled manner as per plan.
4. Once the subgrade is exposed and prior to the placement of any pavement materials or new fill in embankment areas, the top 12 inches of the entire exposed subgrade is to be compacted and proof rolled per 204.03 and Item 204.06, in order to detect any soft, wet or weak zones.
5. If any soft, wet or weak zones are present, as determined by the Engineer, the materials in these zones shall be either scarified, dried, and thoroughly recompact in place in accordance with Item 204, or chemically stabilized per Item 206, or be undercut and replaced per Item 204.04 using Item 204 Granular Material, Type B or C as directed by the Engineer. Geotextile fabric, type D shall be placed at the bottom of the overexcavation.
6. Proof roll the undercut areas according to 204.06 to verify stability.
7. Fine grade the subgrade to the specified grade

REMOVAL AND REPLACEMENT OR STABILIZATION OF SOFT OR UNSUITABLE MATERIAL

Areas requiring over excavation or undercut, as determined by the Engineer, shall be excavated in accordance with Item 204 Excavation of Subgrade. The over excavated or undercut areas are to be filled as directed by the Engineer in a controlled manner in accordance with 204.07 and as follows.

Prior to placement of any new fill or pavement construction, areas of over excavation or undercut are to be replaced with fill comprised of Item 204 Granular Material, Type B or C; or with properly compacted new fill meeting the requirements of Item 204 Embankment; or with excavations scarified to permit drying and recompact to the appropriate design unit weight and sufficient moisture content to meet Item 203 compaction requirements, at the discretion of the Engineer.

The Contractor may propose an alternate means of stabilizing the subgrade provided that the recommendation is made by a qualified geotechnical professional and that the material used in the over excavated area can be tested to verify compaction and moisture content.

The following estimated contingency quantities are carried to the General Summary for use as directed by the Engineer for removal and replacement of soft or unsuitable materials as described above:

Item 204 - Excavation of Subgrade	1,200 Cu. Yd.
Item 204 - Granular Material, Type B	1,200 Cu. Yd.
Item 204 - Geotextile Fabric, Type D	1,690 Sq. Yd.

DRAINAGE CONNECTIONS

The drainage, which are encountered during construction, shall be provided with unobstructed outlets. Existing collectors which are located below the roadway ditch elevations, and which cross the roadway, shall be replaced within the construction limits by Item 611 Conduit, Type B, one commercial size larger than the existing conduit.

Existing collectors and isolated farm drains, which are encountered above the elevation of roadway ditches, shall be outletted into the roadway ditch by 611 Type F Conduit. The optimum outlet elevation shall be one foot above the flowline elevation of the ditch. Lateral field tiles which cross the roadway shall be intercepted by 611, Type E Conduit, and carried in a longitudinal direction to an adequate outlet or roadway crossing.

The location, type, size and grade of replacements shall be determined by the engineer and payment shall be made on final measurements.

Erosion control pads and animal guards shall be provided at the outlet end of all farm drains as per standard construction drawing DM-1.1, except when they outlet into a drainage structure. Payment for the erosion control pads and animal guards and any necessary bends or branches shall be included for payment in the pertinent conduit items.

The following estimated quantities have been included in the General Summary for the work noted above:

ODOT Item 611 - 4" Conduit, Type E for Drainage Connection	100 Ft.
ODOT Item 611 - 6" Conduit, Type E for Drainage Connection	100 Ft.
ODOT Item 611 - 8" Conduit, Type E for Drainage Connection	100 Ft.
ODOT Item 611 - 12" Conduit, Type E for Drainage Connection	100 Ft.

DUST CONTROL

The Contractor shall be responsible for providing dust control measures in accordance with Item 616. Dust Control operations shall be performed on a periodic basis as directed by the Engineer to alleviate or prevent the dust nuisance originating within the project work limits. The following estimated quantities have been included for dust control purposes:

Item 616 - Water	100 M. Gal.
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ITEM 204 - PROOF ROLLING

The following quantity is provided in the General Summary to address locations requiring proof rolling:

Item 204 - Proof Rolling	4 Hours
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ITEM SPECIAL - CLAY LINER

This item of work shall include construction of a clay liner as part of the stormwater management basin at the location indicated on the Plan.

The bottom and sides of the basin shall be overexcavated to an 18" minimum depth and replaced with a compacted clay liner. The liner shall extend to the limits shown on the Plans.

Material for the liner shall be approved by the Engineer prior to the placement of the material.

The Contractor shall be responsible for verifying the integrity of the liner upon completion and acceptance of all items (pipes, headwalls, endwalls, rock channel protection, etc.) that penetrate and/or abut the liner.

The Contractor shall be responsible for verifying the integrity of the liner upon completion and acceptance of all items of work (duct banks, conduits, wiring, etc.) performed by other contractors that penetrate and/or abut the liner.

Payment shall be per cubic yard and shall include all overexcavation, equipment, materials, labor, incidentals, and disposal of materials as necessary to construct the clay liner to the satisfaction of the Engineer.

ITEM SPECIAL - DETENTION OUTLET STRUCTURE

This item of work shall be performed according to CMSC Item 604 and the Plan details. Payment shall be per each and shall include all equipment, labor, materials, and incidentals, including but not limited to catch basin structure, grate, low strength mortar backfill, internal pipe riser, cap, and inlet pipe with associated endwall. Outlet pipe will be paid for under Item Special - Detention Outlet Structure.

ITEM SPECIAL - PERMANENT EROSION CONTROL MATTING

This work consists of furnishing and placing erosion control matting in accordance with CMSC Item 671, as modified herein. (North American Green SC250 or approved equal - https://nagreen.com/sites/default/files/2020-03/GEN_EC_BRO_3.20.pdf).

Preparation: Complete fine grading of the retention basin emergency spillway. Apply Topsoil as indicated on the Plans.

Construction: Install erosion control matting per the manufacturer's specifications and installation guidelines. After the erosion control matting has been installed, then apply the seeding and fertilizer.

Method of Measurement: The Owner will measure Item Special - Permanent Erosion Control Matting by the number of square yards completed and accepted. The City will determine the area based on the surface area covered by the erosion control matting.

The Owner will pay for accepted quantities at the contract price as follows:

Item Special - Permanent Erosion Control Matting Square Yard

ITEM SPECIAL - MAILBOX REMOVED AND RESET

This work shall consist of removing existing mailbox and supports and furnishing and erecting mailbox supports and any associated mounting hardware (which may require temporary relocation during construction and replacement to original location upon completion of construction) in accordance with plan details, and attaching an owner-supplied mailbox at locations specified in the Plan or otherwise established by the Engineer. This work shall also include removal of the existing mailbox and mailbox support during construction.

MATERIALS

Wood posts shall be nominal 4" x 4" square or 4-1/2" diameter round, and conform to Section 710.14. Steel posts shall be nominal pipe size 2" I.D., and conform to AASHTO M 181.

HARDWARE

Plates, screws, bolts, etc. Shall be commercial-grade galvanized steel.

SETTING POSTS

Posts shall be set per the first paragraph of Section 606.03, and shall in no instance be encased in concrete.

MOUNTING BOXES

Supports hardware shall accommodate either a single or a double installation, and no more.

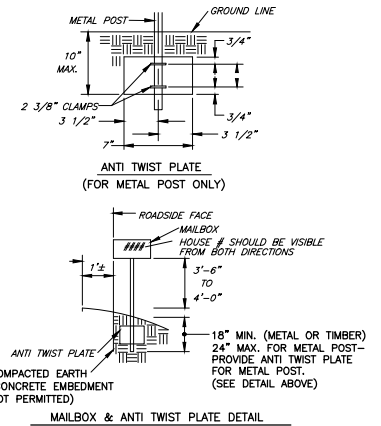
The mailbox shall be securely and neatly attached by the Contractor to the new support. The Contractor shall furnish all necessary attachment hardware (nuts, bolts, plates, spacers and washers) as necessary to accommodate the complete installation.

In the absence of a new box supplied by the Owner, the Contractor shall salvage the existing box and place it on the new support. Due care shall be exercised in such an operation, and the Contractor shall be responsible for repairing or replacing any box damaged by improper handling on his part, as judged and directed by the Engineer.

The Contractor shall be responsible for coordinating with the local post master regarding the timing of the movement of any mailbox to a new location.

Payment under this item shall be limited to final permanent installations. Temporary installations shall be in accordance with section 107.12. However, the same material and size limitations as for permanent installations shall apply.

All tools, labor materials, and incidentals necessary to install, remove, replace and temporarily relocate mailboxes, as stated above, in addition to coordination with landowners or the postal service, shall be included within Item Special - Mailbox Removed and Reset.



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GENERAL NOTES

CHESHIRE ROAD RELOCATION

LEGEND

EXISTING ABBREVIATIONS

WM	WATER MAIN
WS	WATER SERVICE
WSV	WATER SERVICE VALVE
SN	SANITARY SENDER
FM	SANITARY FORCE MAIN
SS	SANITARY SERVICE
GM	GAS MAIN
GS	GAS SERVICE
RD	ROOF DRAIN
OHE	OVERHEAD ELECTRIC
OHC	OVERHEAD COMMUNICATIONS
UGD	UNDERGROUND DUCT BANK
UCD	UNDERGROUND DUCT BANK
CATV	CABLE TELEVISION
CO	CLEAN OUT
E/P	EDGE OF TRAVELED WAY
E/S	EDGE OF SHOULDER
F/C	FACE OF CURB
C&G	CURB & GUTTER

EXISTING CONDITIONS

---	CENTERLINE SURVEY
---	CENTERLINE EXIST. DRIVE
---	EDGE OF PAVEMENT
---	CURB
---	SHOULDER
---	DRIVEWAY
---	PARKING LOT
---	CONC. WALK
---	RIGHT OF WAY
---	PROPERTY LINE
---	PROPERTY LINE EXTENDED
---	LOT LINE (SAME OWNER BOTH SIDES)
---	RAILROAD RIGHT OF WAY
12" / 12"	STORM
12" / SAN 12"	SANITARY
12" / FM 12"	SANITARY FORCE MAIN
12" / WM 12"	WATER MAIN
12" / GW 12"	GAS MAIN
---	ELECTRIC CONDUIT
---	TELEPHONE CONDUIT
---	FIBER OPTIC CONDUIT
---	ELECTRIC DUCT BANK
---	ELECTRIC DUCT BANK
---	DATV
---	DATV CONDUIT
---	OHE-OHC
---	OHE-OHC
---	TEL/COMM CONDUIT

EXISTING UTILITY SYMBOLS

	DITCH / STREAM
	CORPORATION LINE
	FENCE
	WOOD FENCE
	GUARDRAIL
	CONC. WALL
	ROCK
	TREENLINE
	TREES
	BUSHES
	HEDGE/SHRUB
	STUMP
	CONC. PAD
	WOOD POST
	POST/BALLARD
	MANHOLE
	FLAGPOLE
	PARKING BLOCK
	BUILDING

EXISTING SURVEY SYMBOLS

	IRON PIN FND./METAL BAR FND.
	PK. NAIL FND.
	RIGHT-OF-WAY MON. FND.
	IRON ROD FND./RR SPIKE FND.
	BENCHMARK
	IRON PIN SET/MAG. NAIL SET/PK. NAIL SET/RR SPIKE SET/INVERSE DRILL HOLE

EXISTING UTILITY SYMBOLS

	LIGHT POLE
	TELEPHONE POLE
	TELEPHONE LIGHT POLE
	TELEPHONE ELECTRIC POLE
	ELECTRIC POLE
	LIGHT POLE
	GUY ANCHOR

PROPOSED UTILITY SYMBOLS

	STM. MH
	CURB & GUTTER INLET
	CURB INLET
	SAN. MH
	END WALL
	END OF PIPE
	CATCH BASIN
	WATER VALVE
	FIRE HYDRANT
	LIGHT POLE
	ELEC. MANHOLE
	COMM. MANHOLE
	COMM. MANHOLE
	COMM. MANHOLE

PROPOSED ABBREVIATIONS

(DN2)	DO NOT DISTURB
(TBA)	TO BE ABANDONED
(TBR)	TO BE REMOVED
(TBR1)	TO BE RELOCATED BY THIS PROJECT
(TBR2)	TO BE RELOCATED (BY OTHERS)
(TBR3)	TO BE REMOVED (BY OTHERS)
(APP)	AS PER PLAN
(ADJ)	ADJUST TO GRADE
(RTR)	RECONSTRUCT TO GRADE
(RAR)	REMOVE AND RESET
(BO)	BY OTHERS
(BSP)	BY SEPARATE PLAN
(SUP)	SHARED USE PATH
(PA)	PREVIOUSLY ABANDONED
E/W	EDGE OF TRAVELED WAY
E/P	EDGE OF PAVEMENT
E/S	EDGE OF SHOULDER
F/C	FACE OF CURB
C&G	CURB & GUTTER
@	BASELINE

PROPOSED LAYOUT

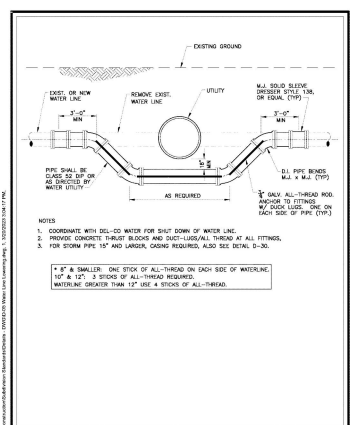
---	CENTERLINE CONSTRUCTION
---	CENTERLINE PROP. DRIVE
---	EDGE OF PAVEMENT
---	BACK OF CURB
---	FACE OF CURB
---	CONC. WALK/ASPH. BIKEPATH
R/W	RIGHT-OF-WAY
SW	STORM EASEMENT
U	UTILITY EASEMENT
SL	SLOPE EASEMENT
TMP	TEMPORARY EASEMENT
---	PROPERTY LINE
---	CONSTRUCTION LIMITS
X	CHAIN LINK / WIRE WOVEN FENCE

CROSS SECTIONS

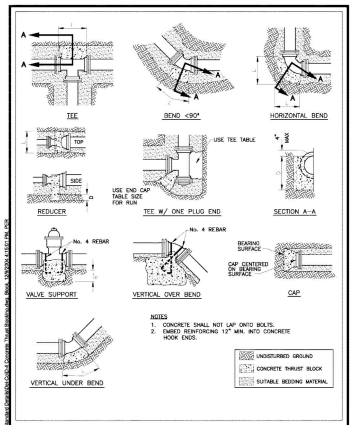
12" / 12"	STORM
12" / 12"	SANITARY
12" / 12"	WATER MAIN
8" / 8"	GAS MAIN
---	TRAFFIC INTERCONNECT
---	COMMUNICATION
---	SIGNAL CONDUIT
---	CENTERLINE OF DITCH
---	DECORATIVE RAILING
---	GUARDRAIL

Proposed Grade @ Const.
806.32
109+50.00 @ Station
797.08
Ex. Grade @ Const.

2/15/2025 11:48 AM User: JJB Date Plotted: 2/15/2025 11:48 AM Plot File: CHESHIRE ROAD RELOCATION.dwg



DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 1 OF 1 D-5



DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 1 OF 2 D-4

END GAP BLOCKING

SIZE	2"	4"	6"	8"	12"	16"
BLOCKING LENGTH	6"x6"	6"x6"	13"x13"	17"x17"	24"x24"	33"x33"

BLOCKING FOR TEES

SIZE	4"	6"	8"	12"	16"
1" Ø	1.1	1.8	2.8	4.8	7.8
1.5" Ø	1.8	3.0	4.8	8.4	13.8
2" Ø	3.0	5.1	8.4	14.7	24.6
3" Ø	5.1	8.4	14.7	26.4	45.0
4" Ø	8.4	14.7	26.4	45.0	78.0
6" Ø	14.7	26.4	45.0	78.0	138.0
8" Ø	26.4	45.0	78.0	138.0	234.0
12" Ø	45.0	78.0	138.0	234.0	414.0
16" Ø	78.0	138.0	234.0	414.0	720.0

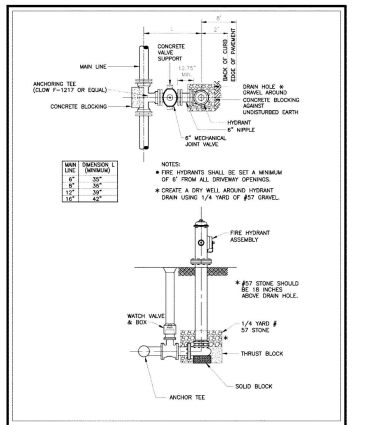
SIZE OF CONCRETE BLOCKING FOR THE MAIN UNDERBEND

PIPE SIZE	4"	6"	8"	12"	16"
1" Ø	1.1	1.8	2.8	4.8	7.8
1.5" Ø	1.8	3.0	4.8	8.4	13.8
2" Ø	3.0	5.1	8.4	14.7	24.6
3" Ø	5.1	8.4	14.7	26.4	45.0
4" Ø	8.4	14.7	26.4	45.0	78.0
6" Ø	14.7	26.4	45.0	78.0	138.0
8" Ø	26.4	45.0	78.0	138.0	234.0
12" Ø	45.0	78.0	138.0	234.0	414.0
16" Ø	78.0	138.0	234.0	414.0	720.0

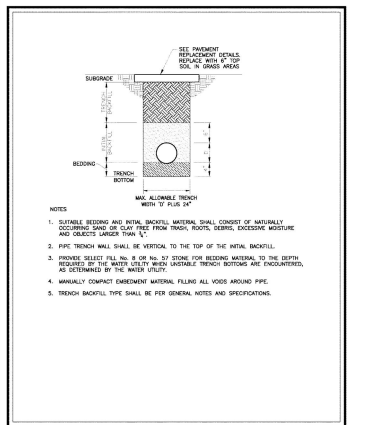
BLOCKING FOR HORIZONTAL BENDS AND VERTICAL UNDERBENDS

PIPE SIZE	11.25"	22.5"	45"	90"	VALUE	MIN	MAX
1" Ø	1.1	1.8	2.8	4.8	16"	0.3	0.3
1.5" Ø	1.8	3.0	4.8	8.4	16"	0.3	0.3
2" Ø	3.0	5.1	8.4	14.7	16"	0.3	0.3
3" Ø	5.1	8.4	14.7	26.4	16"	0.3	0.3
4" Ø	8.4	14.7	26.4	45.0	16"	0.3	0.3
6" Ø	14.7	26.4	45.0	78.0	16"	0.3	0.3
8" Ø	26.4	45.0	78.0	138.0	16"	0.3	0.3
12" Ø	45.0	78.0	138.0	234.0	16"	0.3	0.3
16" Ø	78.0	138.0	234.0	414.0	16"	0.3	0.3

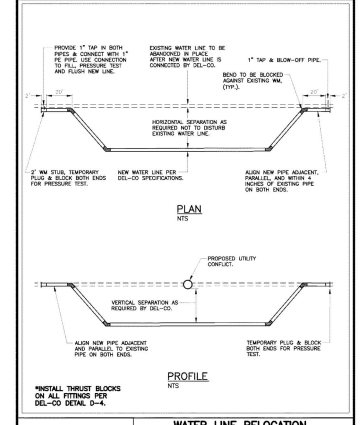
DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 2 OF 2 D-4



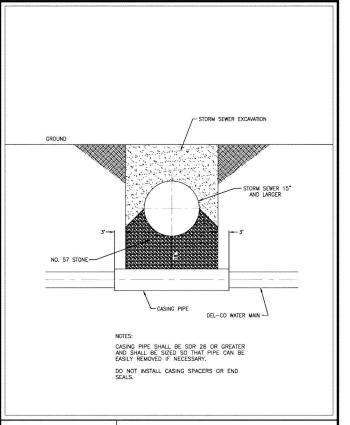
DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 1 OF 1 D-2



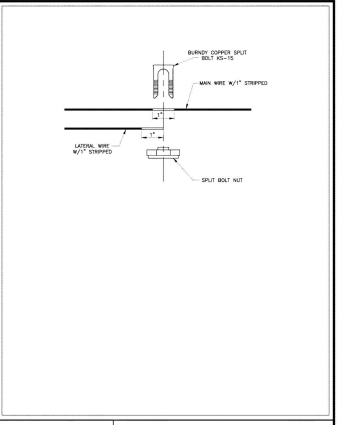
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DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
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PAGE 1 OF 1 D-1



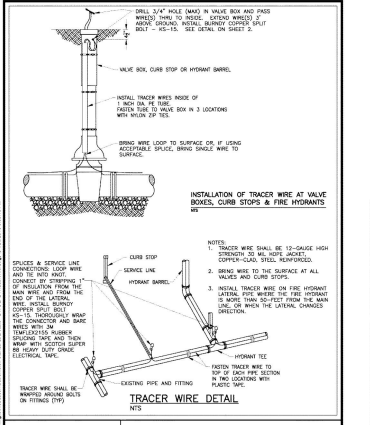
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PROJECT: CHESHIRE ROAD
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PAGE 1 OF 1 D-35



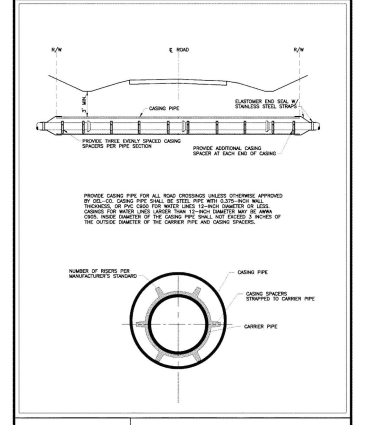
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DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
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PAGE 1 OF 1 D-30



DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 2 OF 2 D-24



DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 1 OF 2 D-24



DEI/CO DEL-CO WATER WORKS
DESIGNED BY AUSTIN
PROJECT: CHESHIRE ROAD
DATE: 01-02-14
SCALE: 1/8"=1'-0"
PAGE 1 OF 1 D-8

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SITE DESCRIPTION

PROJECT NAME AND LOCATION:
CHESHIRE ROAD RELOCATION
CITY OF SUNBURY, DELAWARE COUNTY OHIO

OWNER NAME AND ADDRESS:

SITE CONTACT: JIM JOHLEN
ROMANELLI AND HUGHES
148 W SCHROCK ROAD
WESTERVILLE, OH 43081
Tel: 614-891-2042
Email: JOHLEN@RH-HOMES.COM

DESCRIPTION: THIS PROJECT CONSISTS OF RELOCATING 600' OF CHESHIRE ROAD, RECONSTRUCTING 500' OF CHESHIRE ROAD, AND CONSTRUCTING A NEW ROAD (750') WEST OF THE RELOCATED CHESHIRE ROAD. THE PROJECT INCLUDES ASPHALT PAVEMENT, CONCRETE MEDIAN, CURB AND GUTTER, CURB RAMPS, WATER MAIN, STORM SEWER, SIGNAL, LIGHTING, AND TRAFFIC CONTROL.

THE MAJORITY OF THE EXISTING SITE CONSISTS OF EXISTING ROADWAY, DITCHES, AND OPEN SPACE. THE SITE IS BORDERED BY SUBDIVISIONS TO THE SOUTH, WEST, AND EAST AND COMMERCIAL DEVELOPMENT TO THE NORTH.

STORMWATER RUNOFF FROM THE SITE FLOWS INTO AN UNNAMED TRIBUTARY TO LITTLE WALNUT CREEK.

RUNOFF CURVE NUMBER:
PRE-DEVELOPMENT RUN-OFF CURVE NUMBER = 0.57
POST-DEVELOPMENT RUN-OFF CURVE NUMBER = 0.66

SITE AREA:
THE SITE IS APPROXIMATELY 9.50 ACRES. A TOTAL NOI DISTURBANCE OF 5.50 ACRES WILL BE DISTURBED BY CONSTRUCTION ACTIVITIES ASSOCIATED WITH ROADWAY IMPROVEMENTS.

SITE DESCRIPTION: (CHECK ONE)

- SUBDIVISION _____
- COMMERCIAL _____
- INDUSTRIAL _____
- P.U.D. _____
- OTHER (ROADWAY IMPROVEMENTS)

SOIL TYPES:

- BaA Bennington silt loam, 0 to 2 percent slopes 3.9%
- BaB Bennington silt loam, 2 to 6 percent slopes 17.0%
- Crd1 B1 Cardinton silt loam, 2 to 6 percent slopes 21.1%
- PwA Pewarda Siltly Clay Loam, 0 to 1 percent slopes 8.1%
- UdB Udorthents, clayey-Urban land complex, undulating 49.9%

SEQUENCE OF MAJOR ACTIVITIES:

1. ESTABLISH A TEMPORARY STABILIZED CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT AREA.
2. INSTALL INLET CONTROL ON EXISTING STRUCTURES, FILTER FABRIC DITCH CHECKS, AND FILTER FABRIC SOCKS.
3. CLEAR AND GRUB SITE. COMMENCE WITH GRADING OF THE SITE.
4. INSTALL INLET CONTROL ON PROPOSED STRUCTURES AS THEY ARE CONSTRUCTED.
5. COMMENCE WITH PAVING ACTIVITIES.
6. STABILIZE DISTURBED AREAS ACCORDING TO THE TEMPORARY AND PERMANENT SEEDING REQUIREMENTS.
7. REMOVED TEMPORARY EROSION CONTROLS UPON ESTABLISHMENT OF PERMANENT VEGETATION.

NAME OF RECEIVING WATERS:
STORMWATER RUNOFF FROM THE SITE FLOWS INTO AN UNNAMED TRIBUTARY TO LITTLE WALNUT CREEK.

NPDES PERMIT:

GENERAL NOTES

ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH ALL LOCAL EROSION SEDIMENT CONTROL, WASTE DISPOSAL, SANITARY AND HEALTH REGULATIONS.

ALL EROSION AND SEDIMENT CONTROL PRACTICES MUST MEET THE STANDARDS AND SPECIFICATIONS OF THE OHIO RAINWATER AND LAND DEVELOPMENT HANDBOOK (2006).

OTHER EROSION CONTROL ITEMS MAY BE NECESSARY DUE TO ENVIRONMENTAL CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION AND IMPLEMENTATION OF ADDITIONAL EROSION CONTROL ITEMS, AT THE ENGINEER'S DISCRETION.

REGULAR INSPECTION AND MAINTENANCE WILL BE PROVIDED FOR ALL EROSION AND SEDIMENT CONTROL PRACTICES.

THE CONTRACTOR SHALL USE EROSION CONTROL MEASURES AS NECESSARY TO PREVENT SEDIMENT MOVEMENT INTO AREAS DESIGNATED AS WETLANDS.

NO SOLID OR LIQUID WASTE SHALL BE DISCHARGED INTO STORM WATER RUNOFF.

ADDITIONAL EROSION AND SEDIMENT CONTROL BMP'S MAY BE REQUIRED AS IDENTIFIED BY THE CITY.

STORMWATER LEGISLATION 935 WAS APPROVED IN 2025. REVIEW AND FOLLOW THE MOST UP-TO-DATE VERSION FOR FEES, INSPECTIONS, ENFORCEMENT, AND PENALTIES.

THE CITY MAY ACCESS AND INSPECT MAINTENANCE DURING CONSTRUCTION AND POST-CONSTRUCTION AT ANY TIME, UNANNOUNCED, AND REQUIRE REMEDIATIONS AND/OR MAINTENANCE.

POST-CONSTRUCTION MAINTENANCE REPORTING MUST BE SUBMITTED TO THE CITY OF SUNBURY ANNUALLY BY JANUARY 1ST OF EVERY YEAR IN COMPLIANCE WITH THE DESIGN PLAN AND/OR MANUFACTURER'S SPECIFICATIONS. POST-CONSTRUCTION STORMWATER QUALITY DEVICES/LOCATIONS REQUIRE ANNUAL ODEPA REPORTING.

MINIMIZE OFF SITE SEDIMENT TRACKING OF VEHICLES USING STONE MATERIAL IN ALL CONSTRUCTION ENTRANCES, INCLUDING INDIVIDUAL LOT ACCESS POINTS. STABILIZED CONSTRUCTION ENTRANCES TO BE PROPERLY MAINTAINED AND IN GOOD WORKING ORDER, AT ALL TIMES. THIS MAY REQUIRE PERIODIC TOP DRESSING OF STONE AS CONDITIONS AND USE DEMAND. IN ADDITION, MANUAL REMOVAL OF SEDIMENT AND DEBRIS FROM THE STREETS AND GUTTERS, AFTER EACH WORKDAY, MAY BE NECESSARY TO ACHIEVE SATISFACTORY RESULTS. IF WEATHER CONDITIONS AND USE OF STONE CONSTRUCTION ENTRANCE DOES NOT PRODUCE SATISFACTORY RESULTS, ENHANCED METHODS SUCH AS SHAKER RACKS AND/OR TIRE WASH CLEANING SYSTEMS MAY BE REQUIRED.

AT THE REQUIRED PRE-CONSTRUCTION MEETING WITH THE CITY, INCLUDE TRUCK ROUTES AND GENERAL DEVELOPMENT BUILDOUT IF POSSIBLE.

DESIGNER ENGINEER WILL PROVIDE THE CITY WITH CAD/GIS AS THEY ARE ABLE, FOR THE CITY'S THIRD PARTY INSPECTIONS OF SITE COMPLIANCE TRACKING.

REGULARLY SCHEDULED STREET SWEEPING USING A COMBINATION AND FREQUENCY OF SCRAPING, ROTARY BOX BROOM/PICKUP SWEEPER AND ROTARY BRUSH VACUUM EQUIPMENT MAY BE REQUIRED TO ACHIEVE SATISFACTORY RESULTS.

CONTROLS
EROSION AND SEDIMENT CONTROLS:
STABILIZATION PRACTICES

TEMPORARY STABILIZATION - TOP SOIL STOCK PILES AND DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR AT LEAST 14 DAYS WILL BE STABILIZED WITH TEMPORARY SEED AND MULCH NO LATER THAN FOURTEEN (14) DAYS FROM THE LAST CONSTRUCTION ACTIVITY IN THAT AREA. THE TEMPORARY SEED SHALL BE APPLIED AS PER THE TEMPORARY SEEDING SPECIFICATIONS. AREAS OF THE SITE WHICH ARE TO BE PAVED WILL BE TEMPORARILY STABILIZED BY APPLYING GEOTEXTILE AND STONE SUB-BASE UNTIL ASPHALT PAVEMENT CAN BE APPLIED.

PERMANENT STABILIZATION - DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES PERMANENTLY CEASES SHALL BE STABILIZED WITH PERMANENT SEED NO LATER THAN SEVEN (7) DAYS AFTER THE LAST CONSTRUCTION ACTIVITY OR WITHIN TWO (2) DAYS FOR AREAS WITHIN FIFTY FEET (50') OF A STREAM. REFER TO LANDSCAPE PLAN FOR DETAILS.

STABILIZATION TYPE	J	F	M	A	M	J	J	A	S	O	N	D
PERMANENT SEEDING	●	●	●	●	●	●	●	●	●	●	●	●
DOMANT SEEDING	●	●	●	●	●	●	●	●	●	●	●	●
TEMPORARY SEEDING	●	●	●	●	●	●	●	●	●	●	●	●
SODDING	●	●	●	●	●	●	●	●	●	●	●	●
MULCHING	●	●	●	●	●	●	●	●	●	●	●	●

● - IRRIGATION NEEDED
●● - IRRIGATION NEEDED FOR 2-3 WEEKS AFTER SOO IS APPLIED

OTHER CONTROLS
WASTE DISPOSAL:

ALL WASTE MATERIALS WILL BE COLLECTED AND STORED IN A SECURELY LOADED METAL DUMPSTER RENTED FROM A LICENSED SOLID WASTE MANAGEMENT COMPANY. THE DUMPSTER WILL MEET ALL LOCAL, CITY AND STATE SOLID WASTE MANAGEMENT REGULATIONS. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE WILL BE DEPOSITED IN THE DUMPSTER. THE DUMPSTER WILL BE EMPTIED A MINIMUM OF TWICE PER WEEK OR MORE OFTEN IF NECESSARY, AND THE TRASH WILL BE HAULED OFF-SITE. NO CONSTRUCTION WASTE MATERIALS WILL BE BURIED ON-SITE. ALL PERSONNEL WILL BE INSTRUCTED REGARDING THE CORRECT PROCEDURE FOR WASTE DISPOSAL. NOTICES STATING THESE PRACTICES WILL BE POSTED IN THE OFFICE TRAILER. THE INDIVIDUAL WHO MANAGES THE DAY-TO-DAY SITE OPERATIONS WILL BE RESPONSIBLE FOR SEEING THAT THESE PROCEDURES ARE FOLLOWED. ALL CONSTRUCTION AND DEMOLITION DEBRIS (C&DD) WASTE WILL BE DISPOSED OF IN AN OHIO EPA APPROVED C&DD LANDFILL AS REQUIRED BY ORC 371.4

HAZARDOUS WASTE:

ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN THE MANNER SPECIFIED BY LOCAL OR STATE REGULATION OR BY THE MANUFACTURER. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES. THE INDIVIDUAL WHO MANAGES DAY-TO-DAY SITE OPERATIONS WILL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED.

SANITARY WASTE:

ALL SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS A MINIMUM OF THREE TIMES PER WEEK BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR, AS REQUIRED BY LOCAL REGULATION.

OFF-SITE VEHICLE TRACKING:

OFF-SITE TRACKING OF SEDIMENTS SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION ENTRANCE WILL BE PROVIDED TO HELP REDUCE VEHICLE TRACKING OF SEDIMENTS. ALL PAVED STREETS ADJACENT TO THE SITE WILL BE SWEEPED DAILY TO REMOVE ANY EXCESS MUD, DIRT OR ROCK TRACKED FROM THE SITE. DUMP TRUCKS HAULING MATERIAL FROM THE CONSTRUCTION SITE WILL BE COVERED WITH A TARPULIN.

DEWATERING ACTIVITIES:

THERE SHALL BE NO TURBID DISCHARGES TO SURFACE WATERS, RESULTING FROM DEWATERING ACTIVITIES. SEDIMENT-LADEN WATER MUST PASS THROUGH A SETTLING POND, FILTER BAG, OR OTHER COMPARABLE PRACTICE, PRIOR TO DISCHARGE.

PROCESS WASTEWATER:

ALL PROCESS WASTEWATER (EQUIPMENT WASHING, LEACHATE FROM ON-SITE WASTE DISPOSAL, ETC.) SHALL BE COLLECTED AND DISPOSED OF AT A PUBLICLY OWNED TREATMENT WORKS.

TIMING OF CONTROLS/MEASURES

AS INDICATED IN THE SEQUENCE OF MAJOR ACTIVITIES, CONSTRUCTION ENTRANCES) AND SILT FENCE WILL BE CONSTRUCTED PRIOR TO CLEARING OR GRADING OF ANY OTHER PORTIONS OF THE SITE. SEDIMENT CONTROL DEVICES SHALL BE IMPLEMENTED FOR ALL AREAS REMAINING DISTURBED LONGER THAN FOURTEEN (14) DAYS AND/OR WITHIN SEVEN (7) DAYS OF ANY GRUBBING ACTIVITIES. AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR MORE THAN FOURTEEN (14) DAYS WILL BE STABILIZED WITH A TEMPORARY SEED AND MULCH WITHIN TWO (2) DAYS OF THE LAST DISTURBANCE IF THE AREA IS WITHIN FIFTY FEET (50') OF A STREAM, AND WITHIN SEVEN (7) DAYS OF THE LAST DISTURBANCE IF THE AREA IS MORE THAN FIFTY FEET (50') AWAY FROM A STREAM. ONCE CONSTRUCTION ACTIVITY CEASES PERMANENTLY IN AN AREA, THAT AREA WILL BE STABILIZED WITH PERMANENT SEED AND MULCH. AFTER THE ENTIRE SITE IS STABILIZED, THE ACCUMULATED SEDIMENT WILL BE REMOVED FROM THE BASIN.

SPILL PREVENTION

MATERIAL MANAGEMENT PRACTICES:

THE FOLLOWING ARE THE MATERIAL MANAGEMENT PRACTICES THAT WILL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS AND SUBSTANCES TO STORMWATER RUNOFF.

GOOD HOUSEKEEPING: THE FOLLOWING GOOD HOUSEKEEPING PRACTICES WILL BE FOLLOWED ON-SITE DURING THE CONSTRUCTION PROJECT.

1. AN EFFORT WILL BE MADE TO STORE ONLY ENOUGH PRODUCT REQUIRED TO DO THE JOB.
2. ALL MATERIALS STORED ON-SITE WILL BE STORED IN A NEAT, ORDERLY MANNER IN THEIR APPROPRIATE CONTAINERS AND, IF POSSIBLE, UNDER A ROOF OR OTHER ENCLOSURE.
3. PRODUCTS WILL BE KEPT IN THEIR ORIGINAL CONTAINERS WITH THE ORIGINAL MANUFACTURER'S LABEL.
4. SUBSTANCES WILL NOT BE MIXED WITH ONE ANOTHER UNLESS RECOMMENDED BY THE MANUFACTURER.
5. WHENEVER POSSIBLE, ALL OF A PRODUCT WILL BE USED UP BEFORE DISPOSING OF THE CONTAINER.
6. MANUFACTURERS' RECOMMENDATIONS FOR PROPER USE AND DISPOSAL WILL BE FOLLOWED.
7. THE SITE SUPERINTENDENT WILL INSPECT DAILY TO ENSURE PROPER USE AND DISPOSAL OF MATERIALS ON-SITE.

HAZARDOUS PRODUCTS: THESE PRACTICES ARE USED TO REDUCE THE RISKS ASSOCIATED WITH HAZARDOUS MATERIALS.

1. PRODUCTS WILL BE KEPT IN ORIGINAL CONTAINERS UNLESS THEY ARE NOT RESEALABLE BY THE MANUFACTURER. ONCE APPLIED, THEY CONTAIN IMPORTANT PRODUCT INFORMATION.
2. ORIGINAL LABELS AND MATERIAL SAFETY DATA WILL BE RETAINED; THEY WILL BE FOLLOWED.
3. IF SURPLUS PRODUCT MUST BE DISPOSED OF, MANUFACTURERS' OR LOCAL AND STATE RECOMMENDED METHODS FOR PROPER DISPOSAL WILL BE FOLLOWED.

PRODUCT SPECIFIC PRACTICES

THE FOLLOWING PRODUCT SPECIFIC PRACTICES WILL BE FOLLOWED ON-SITE:

PETROLEUM PRODUCTS - ALL ON-SITE VEHICLES WILL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE. PETROLEUM PRODUCTS WILL BE STORED IN TIGHTLY SEALED CONTAINERS WHICH ARE CLEARLY LABELED. ANY ASPHALT SUBSTANCES USED ON-SITE WILL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

FUEL STORAGE - FUEL STORAGE TANKS SHALL BE LOCATED AWAY FROM SURFACE WATERS AND STORM SEWER SYSTEM INLETS. FUEL TANKS SHALL BE STORED IN A DIKED AREA CAPABLE OF HOLDING 150% OF THE TANK CAPACITY.

FERTILIZERS - FERTILIZERS USED WILL BE APPLIED ONLY IN THE MINIMUM AMOUNTS RECOMMENDED BY THE MANUFACTURER. ONCE APPLIED, FERTILIZER WILL BE WORKED INTO THE SOIL TO LIMIT EXPOSURE TO STORMWATER. STORAGE WILL BE IN A COVERED SHED. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER WILL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.

PAINTS - ALL CONTAINERS WILL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT WILL NOT BE DISCHARGED TO THE STORM SEWER SYSTEM BUT WILL BE PROPERLY DISPOSED OF ACCORDING TO MANUFACTURERS' INSTRUCTIONS OR STATE AND LOCAL REGULATIONS.

CONCRETE TRUCKS - CONCRETE TRUCKS WILL NOT BE ALLOWED TO WASH OUT OR DISCHARGE SURPLUS CONCRETE OR DRUM WASH WATER ON THE SITE.

SPILL CONTROL PRACTICES

IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING PRACTICES WILL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:

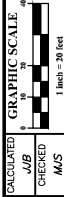
1. ALL SPILLS SHALL BE CLEANED UP IMMEDIATELY AFTER DISCOVERY. MANUFACTURERS' RECOMMENDED METHODS FOR SPILL CLEANUP POSTED AND SITE PERSONNEL WILL BE MADE AWARE OF THE PROCEDURES AND THE LOCATION OF THE INFORMATION AND CLEANUP SUPPLIES.
2. MATERIALS AND EQUIPMENT NECESSARY FOR SPILL CLEANUP WILL BE KEPT IN THE MATERIAL STORAGE AREA ON-SITE. EQUIPMENT AND MATERIALS WILL INCLUDE BUT NOT BE LIMITED TO BROOMS, DUST PANS, MOPS, RAGS, GLOVES, GOGGLES, KITTY LITTER, SAND, SANDUST, AND PLASTIC AND METAL TRASH CONTAINERS SPECIFICALLY FOR THIS PURPOSE.
3. THE SPILL AREA WILL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
4. SPILLS OF TOXIC OR HAZARDOUS MATERIAL WILL BE REPORTED TO THE APPROPRIATE STATE OR LOCAL GOVERNMENT AGENCY. RECORDS OF THE SIZE, SPILLS OF 25 OR MORE GALLONS OF PETROLEUM WASTE MUST BE REPORTED TO OHIO EPA (1-800-282-9378), THE LOCAL FIRE DEPARTMENT, AND THE LOCAL EMERGENCY PLANNING COMMITTEE WITHIN THIRTY (30) MINUTES OF THE SPILL.
5. SOILS CONTAMINATED BY PETROLEUM OR OTHER CHEMICAL SPILLS MUST BE TREATED/DISPOSED AT AN OHIO EPA APPROVED SOLID WASTE MANAGEMENT FACILITY OR HAZARDOUS WASTE TREATMENT, STORAGE OR DISPOSAL FACILITY (TSDF).
6. THE SPILL PREVENTION PLAN WILL BE ADJUSTED TO INCLUDE MEASURES TO PREVENT THIS TYPE OF SPILL FROM REOCCURRING AND HOW TO CLEAN UP THE SPILL IF THERE IS ANOTHER ONE. A DESCRIPTION OF THE SPILL, WHAT CAUSED IT, AND THE CLEANUP MEASURES WILL ALSO BE INCLUDED.
7. THE SITE SUPERINTENDENT RESPONSIBLE FOR THE DAY-TO-DAY SITE OPERATIONS, WILL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. HE WILL DESIGNATE SITE PERSONNEL WHO WILL RECEIVE SPILL PREVENTION AND CLEANUP TRAINING. THESE INDIVIDUALS WILL EACH BECOME RESPONSIBLE FOR A PARTICULAR PHASE OF PREVENTION AND CLEANUP. THE NAMES OF RESPONSIBLE SPILL PERSONNEL WILL BE POSTED IN THE MATERIAL STORAGE AREA AND IN THE OFFICE TRAILER ON-SITE.

DUST CONTROL

DUST CONTROL INVOLVES PREVENTING OR REDUCING DUST FROM EXPOSED SOILS OR OTHER SOURCES DURING LAND DISTURBANCE, DEMOLITION AND CONSTRUCTION ACTIVITIES TO REDUCE THE PRESENCE OF AIRBORNE SUBSTANCES WHICH MAY PRESENT HEALTH HAZARDS, TRAFFIC SAFETY PROBLEMS OR HARM ANIMAL OR PLANT LIFE.

THE FOLLOWING SPECIFICATIONS FOR DUST CONTROL SHALL BE FOLLOWED ON-SITE:

1. **VEGETATIVE COVER AND MULCH** - APPLY TEMPORARY OR PERMANENT SEEDING AND MULCH TO AREAS THAT WILL REMAIN IDLE FOR OVER TWENTY ONE (21) DAYS. SAVING EXISTING TREES AND LARGE SHRUBS WILL ALSO REDUCE SOIL AND AIR MOVEMENT ACROSS DISTURBED AREAS. SEE TEMPORARY SEEDING; PERMANENT SEEDING; MULCHING PRACTICES; AND TREE AND NATURAL AREA PROTECTION PRACTICES.
2. **WATERING** - SPRAY SITE WITH WATER UNTIL THE SURFACE IS WET BEFORE AND DURING GRADING AND REPEAT AS NEEDED, ESPECIALLY ON HAUL ROADS AND OTHER HEAVY TRAFFIC ROUTES. WATERING SHALL BE DONE AT A RATE THAT PREVENTS DUST BUT DOES NOT CAUSE SOIL EROSION. WETTING AGENTS SHALL BE UTILIZED ACCORDING TO MANUFACTURERS INSTRUCTIONS.
3. **SPRAY-ON ADHESIVES** APPLY ADHESIVE ACCORDING TO THE FOLLOWING TABLE OR MANUFACTURERS' INSTRUCTIONS.
4. **STONE** - GRADED ROADWAYS AND OTHER STABLE AREAS WILL BE STABILIZED USING CRUSHED STONE OR COARSE GRAVEL AS SOON AS PRACTICABLE AFTER REACHING AN INTERIM OR FINAL GRADE. CRUSHED STONE OR COARSE GRAVEL CAN BE USED AS A PERMANENT COVER TO PROVIDE CONTROL OF SOIL EMISSIONS.
5. **BARRIERS** - EXISTING WINDBREAK VEGETATION SHALL BE MARKED AND PRESERVED. SNOW FENCING OR OTHER SUITABLE BARRIER MAY BE PLACED PERPENDICULAR TO PREVAILING AIR CURRENTS AT INTERVALS OF ABOUT FIFTEEN (15) TIMES THE BARRIER HEIGHT TO CONTROL AIR CURRENTS AND BLOWING SOIL.
6. **CALCIUM CHLORIDE** - THIS CHEMICAL MAY BE APPLIED BY MECHANICAL SPREADER AS LOOSE, DRY GRANULES OR FLAKES AT A RATE THAT KEEPS THE SURFACE MOIST BUT NOT SO HIGH AS TO CAUSE WATER POLLUTION OR PLANT DAMAGE. APPLICATION RATES SHOULD BE STRICTLY IN ACCORDANCE WITH SUPPLIERS' SPECIFIED RATES.
7. **OPERATION AND MAINTENANCE** - WHEN TEMPORARY DUST CONTROL MEASURES ARE USED, REPETITIVE TREATMENT SHOULD BE APPLIED AS NEEDED TO ACHIEVE DUST CONTROL.
8. **STREET CLEANING** - PAVED AREAS THAT HAVE ACCUMULATED SEDIMENT FROM CONSTRUCTION SHOULD BE CLEANED DAILY, OR AS NEEDED, UTILIZING A STREET SWEEPER OR BUCKET-TYPE ENDLOADER OR SCRAPER.



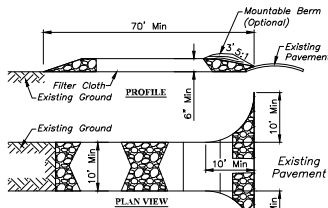
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SEDIMENT & EROSION CONTROL NOTES

CHESHIRE ROAD RELOCATION

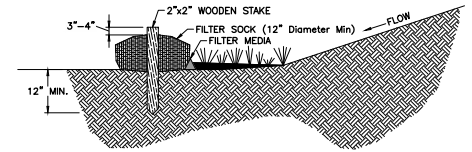
CONSTRUCTION SPECIFICATION NOTES

1. Stone Size – Use 2" stone, or reclaimed or recycled concrete equivalent.
2. Length – Seventy (70) foot minimum.
3. Thickness – Not less than six (6) inches.
4. Width – Twenty (20) foot minimum, but not less than the full width at points where ingress or egress occurs.
5. Geotextile – Shall be placed over the entire area prior to placing of stone.
6. Surface Water – All surface water flowing or diverted toward construction entrances shall be piped across the entrance. If piping is impractical, a mountable berm with 5:1 slopes will be permitted. Cost of pipe shall be included in the price bid for the Stabilized Construction Entrance.
7. Maintenance – The entrance shall be maintained in a condition which will prevent tracking or flowing of sediment onto public right-of-way. This may require periodic top dressing with additional stone as conditions demand and repair and/or cleanout of any measures used to trap sediment. All sediment spilled, dropped, washed or tracked onto public rights-of-way shall be removed immediately.
8. Washing – Wheels shall be cleaned to remove sediment prior to entrance onto public right-of-ways. When washing is required, it shall be done on an area stabilized with stone and which drains into an approved sediment trapping device. The Contractor's bid shall include costs associated with staffing and operating the wheel wash station.
9. Periodic inspection and needed maintenance shall be provided after each rain.



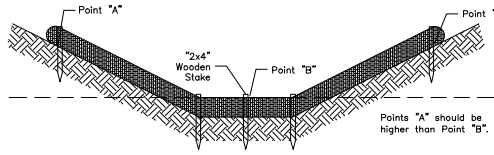
STABILIZED CONSTRUCTION ENTRANCE
NOT TO SCALE

1. Materials – compost used for filter socks shall be weed, pathogen and insect free and free of any refuse, contaminants or other materials toxic to plant growth. They shall be derived from a well-decomposed source of organic matter and consist of a particles ranging from 3/8" to 2".
 2. Filter socks shall be 3 or 5 mil continuous, tubular, hdpe 3/8" knitted mesh netting material, filled with compost passing the above specifications for compost products.
- Installation:**
3. Filter socks will be placed on a level line across slopes, generally parallel to the base of the slope or other affected area. On slopes approaching 2:1, additional socks shall be provided at the top and as needed mid-slope.
 4. Upon installation of the filter sock, additional filter media (matching the media inside of the sock) shall be placed on the upland side of the filter sock. Filter media shall extend halfway up the sock and slope at a maximum of 45 degrees to existing ground elevation.
 5. Filter socks intended to be left as a permanent filter or part of the natural landscape, shall be seeded at the time of installation for establishment of permanent vegetation.
- Maintenance:**
7. Routinely inspect filter socks after each significant rain, maintaining filter socks in a functional condition at all times.
 8. Remove sediments collected at the base of the filter socks when they reach 1/3 of the exposed height of the practice.
 9. Where the filter sock deteriorates or fails, it will be repaired or replaced with a more effective alternative.
 10. Removal – filter socks will be dispersed on site when no longer required in such as way as to facilitate and not obstruct seedings.



FILTER SOCK
Not to Scale

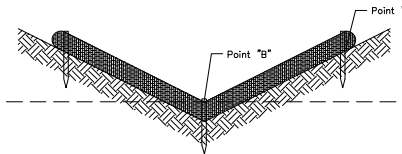
CONSTRUCTION:
Trench the filter fabric fence as detailed for PERIMETER COMPOST FILTER SOCK FENCE/SEDIMENT FENCE. Place a vertical "2x4" stake in the center of the ditch with the top level to the top of the fence and at least 6" below the bottom of the ditch.



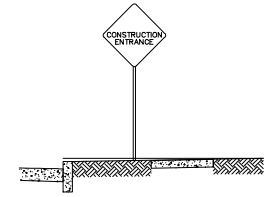
ITEM 207 - FILTER SOCK CHECK DAM
NOT TO SCALE

NOTES:
MATERIALS: Furnish Compost filter sock ditch checks consisting of the following materials:

1. 30" wide Compost filter sock with sound wood supports with maximum on-center spacing of 10'. Use filter fabric conforming to ODOT Item #12.09 Type C.
2. A vertically driven "2x4" stake the center of the ditch.

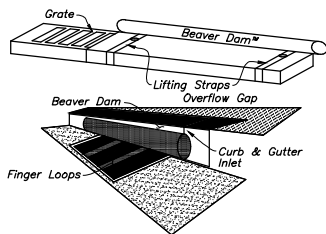


ELEVATION VIEW OF "V" DITCH
SCALE: NONE



CONSTRUCTION ENTRANCE SIGN
NOT TO SCALE

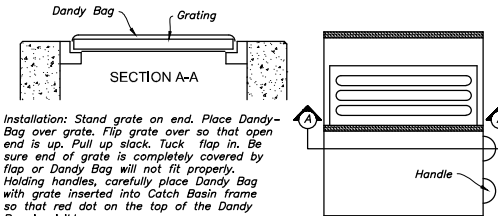
- Notes:**
1. Signs shall be installed in advance of entrance.



ITEM 207 - INLET PROTECTION - THE DANDY CURB BAG
Scale: NONE
Applies to Structure #'s: XX, XX....

Installation: Stand grate on end. Slide the Beaver Dam Bag on with Dam on top of the grate. Pull all excess down. Lay unit on its side. Carefully tuck flap in. Press Velcro strips together. Install the unit making sure front edge of grate is inserted in frame first then lower back into place. Press Velcro dots together which are located under lifting straps. This insures straps remain flush with gutter.

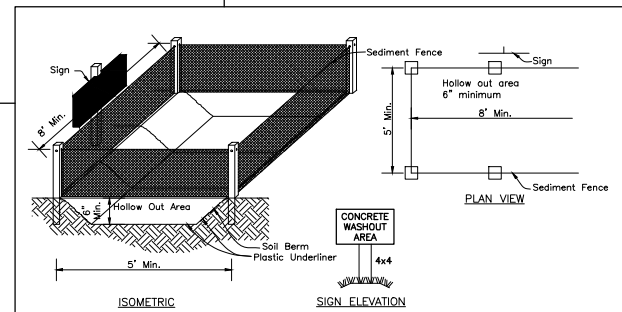
Maintenance: With a stiff bristle broom sweep silt and other debris off surface after each event.



Installation: Stand grate on end. Place Dandy-Bag over grate. Flip grate over so that open end is up. Pull up slack. Tuck flap in. Be sure end of grate is completely covered by flap or Dandy Bag will not fit properly. Holding handles, carefully place Dandy Bag with grate inserted into Catch Basin frame so that red dot on the top of the Dandy Bag is visible.

Maintenance: After silt has dried, remove it from the surface of Dandy Bag with broom.

ITEM 207 - INLET PROTECTION (DANDY BAG)
SCALE: NONE



CONCRETE WASHOUT AREA
NOT TO SCALE

NOTES

1. Concrete trucks shall utilize designated areas to washout trucks.
2. Accumulated concrete shall be removed from the site and disposed of properly.
3. Geotextile – shall be placed over the entire Hollow Out Area prior to use.
4. Provide all items noted above including removal of concrete washout upon completion of the project in Item 207 Concrete Washout, As Per Plan.

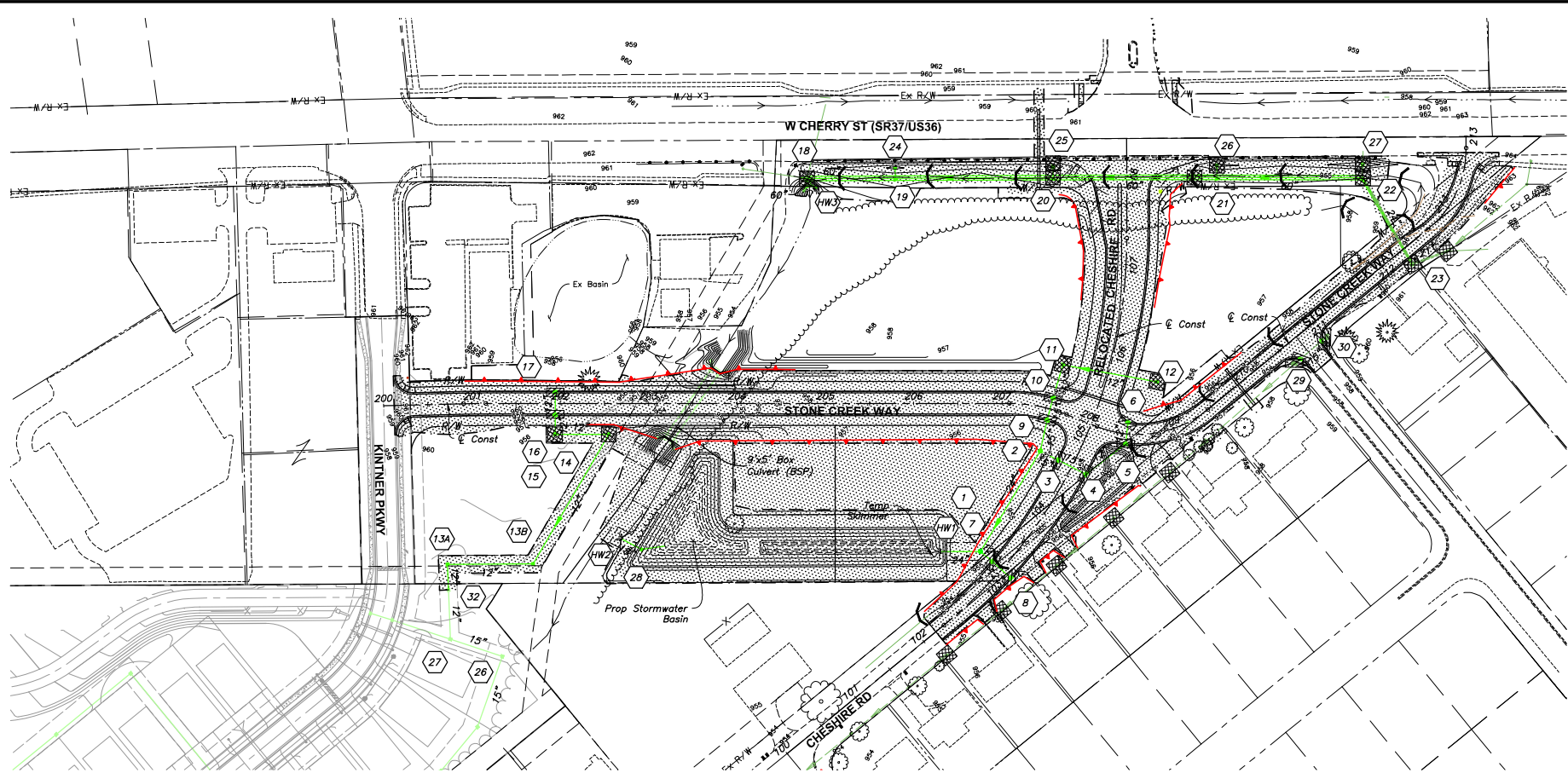
CALCULATED
JUB
CHECKED
MAJ

GRAPHIC SCALE
1 Inch = 20 feet

SEDIMENT & EROSION CONTROL DETAILS

CHESHIRE ROAD RELOCATION

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 P:\2024\09\Cheshire\20240925\20240925.dwg, Job Date: 09/25/2024, 10:17:29 AM, Job: Project: 2024, Job No: 2024, Job Name: 20240925.dwg, Job Date: 09/25/2024, 10:17:29 AM



- Legend**
- Existing Ditch
 - Perimeter Filter Sock
 - Limits of Disturbance
 - Inlet Protection (Beaver Dam) (For detail see sht. 11)
 - Inlet Protection (Dandy Bag) (For detail see sht. 11)
 - Proposed Compost Sock Check Dam
 - Concrete Washout Area (Contractor to determine location and coordinate with City for Approval)
 - Construction Entrance (Contractor to determine location and coordinate with City for Approval)
 - Contractor to coordinate all areas designated for storage or disposal of solid, hazardous, or toxic wastes, including dumpster areas, areas designated for truck washout and vehicle tracking, with City for approval.



GRAPHIC SCALE
 1 inch = 60 feet
 CALCULATED: DAD
 CHECKED: MUS

SEDIMENT & EROSION CONTROL PLAN

CHESHIRE ROAD RELOCATION

ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN
GENERAL

All traffic control devices shall be furnished, erected, maintained and removed by the Contractor in accordance with the "Ohio Manual of Uniform Traffic Control Devices" (Current Edition), copies of which are available from the Ohio Department of Transportation, Office of Traffic Engineering, 1980 West Broad Street, Columbus, Ohio 43223.

The roadway shall not be opened to traffic until permanent traffic controls are in place, or until temporary traffic controls, approved by the Engineer, are installed. The Contractor assumes all liability for the premature removal of temporary traffic controls.

The Contractor shall be responsible for the reinstallation and/or replacement of all permanent traffic control devices damaged or removed during the construction. Permanent traffic control that is no longer in conflict with temporary traffic control shall be replaced immediately. The Contractor shall assume all liability for missing, damaged and improperly placed traffic control devices.

The Contractor shall provide a 24 hour contact who will be responsible for maintenance of traffic for the duration of the project.

Construction operations shall not begin until all temporary traffic control devices are in place and approved by the Engineer and the City.

Access to all adjoining properties shall be maintained at all times. Access for mail delivery, emergency and service vehicles shall not be disrupted. The Contractor shall coordinate construction activities with the Engineer and the owners of the abutting properties in advance (10 Days) of any operations which affect access.

Maintenance of all traffic control devices including drums, signs, barricades, sign boards, detour signage, etc., shall be the Contractor's responsibility.

Drums shall be placed as follows: 35' c/c on tangents, 20' c/c on tapers, and 8' c/c in radii.

A flashing arrow panel (48"x96" - Type "C") shall be used in lane closures in accordance with the Ohio Manual.

All trenches shall be backfilled or securely plated during all nighttime and non-working hours.

Flashing yellow type "B" lights shall be required on all barricades in use at night. All advance signing shall be equipped with type "A" flashing lights. Cones are not approved for use at night. Lights are not required on signs in place during daylight hours.

The Contractor shall be responsible for the protection and safe movement of pedestrians through, around, or detoured away from the construction site. Traffic control for pedestrian movements shall be as per figures TA-28 and TA-29 of Part VI of the Ohio Manual of Uniform Traffic Control Devices.

The safety of pedestrian traffic shall be considered at all times in the provision of traffic control devices required by these plans and notes. It shall be the Contractor's responsibility to provide lights, signs, barricades, and other warnings to physically separate the pedestrian from hazards incidental to the construction operations such as anchor bolts, open excavations, etc.

The Contractor shall submit in writing (a minimum of 14 days prior to construction) a plan to maintain pedestrian traffic for the duration of the project to the City and the Engineer for approval.

For areas adjacent to vehicular traffic, open trench shall be adequately maintained and protected with the proper traffic control devices at all times. Placement of subbase and base material shall follow closely as possible behind excavation operations. The length of trench which is open at any one time shall be held to a length that can be backfilled for nighttime and non-working hours. Drop offs within the work zone shall conform to the requirements set forth on ODOT Standard Construction Drawing MT-101.90

MAINTENANCE OF DRAINAGE

It shall be the Contractor's responsibility to ensure that existing drainage is maintained throughout all phases of construction. Temporary drainage, if required, shall be provided by the Contractor and included with Item 614, Maintaining Traffic, As Per Plan. Special consideration shall be given to maintaining existing drainage during construction of the proposed box culvert.

SEQUENCE OF CONSTRUCTION

The pre-phase shall construct the improvements to the south of W Cherry Street, outside the limits of existing pavement. Traffic shall not be impacted during this phase.

Phase 1 shall construct the new roadway improvements that are proposed outside of the limits of the existing roads. Traffic shall not be impacted during this phase.

Phase 2A shall install a portion of the proposed water main crossing of W Cherry Street via one lane, two way flagging operations per ODOT SCD 97.10. Work shall only be performed Monday through Friday from 9 AM - 3 PM.

Phase 2B shall install the remainder of the proposed water main crossing of W Cherry Street via one lane, two way flagging operations per ODOT SCD 97.10. Work shall only be performed Monday through Friday from 9 AM - 3 PM.

Phase 3 shall construct the new roadway connection of Cheshire Road (relocated) at existing W Cherry Street. The new roadway connection shall be installed by shifting eastbound traffic into the existing center lane per ODOT SCD MT-95.61. Two lane, two way traffic shall be maintained on W Cherry Street at all times, and work shall only be performed Monday through Friday from 9 AM - 3 PM.

Phase 4 shall construct the new roadway connection of Cheshire Road (relocated) at Old Cheshire Road. The new roadway connection shall be installed by closing Old Cheshire Road per ODOT SCD MT-101.60 and the details provided herein. Traffic shall be detoured according to the plan details. The closure shall be limited to 7 days and the Contractor shall phase construction to maintain access to all private properties along Old Cheshire Road at all times.









Phase 5 shall reconstruct Old Cheshire Road. The reconstructed roadway connection at Old Cheshire Road and W Cherry Street shall be installed by shifting eastbound traffic into the existing center lane per ODOT SCD MT-95.61. Two lane, two way traffic shall be maintained on W Cherry Street at all times, and work shall only be performed Monday through Friday from 9 AM - 3 PM. The Contractor shall phase construction to maintain access to all private properties along Old Cheshire Road at all times.

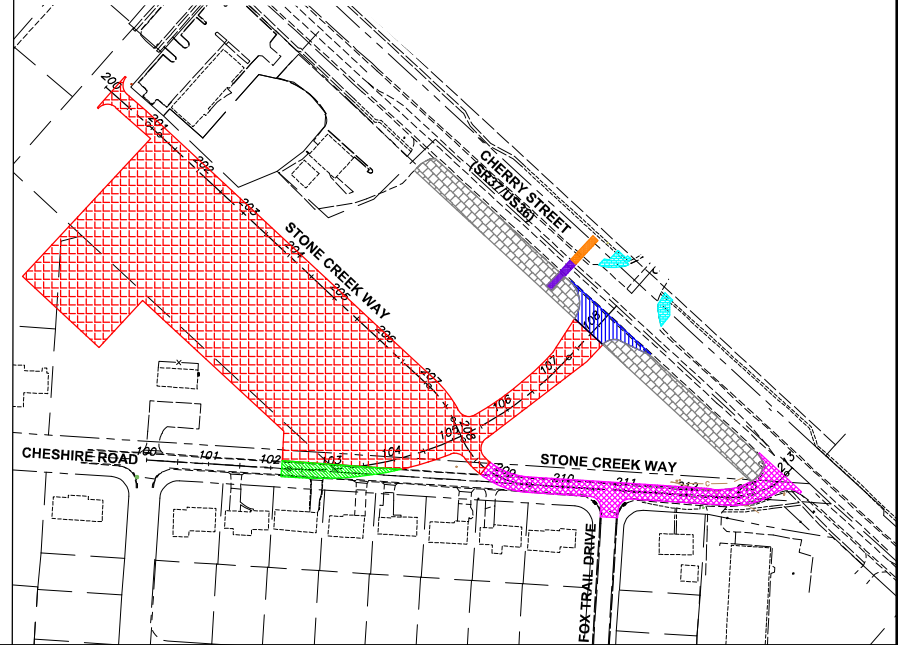
Phase 6 shall construct the new sidewalk improvements that are proposed on the northeast and northwest corners of W Cherry Street and Cheshire Road (relocated). Traffic shall not be impacted during this phase.

	1	2A	2B	3	4	5	6	Pre-Phase
1	X	X	X	X	X	X	X	X
2A	Yes	X	X	X	X	X	X	X
2B	Yes	No	X	X	X	X	X	X
3	Yes	No	No	X	X	X	X	X
4	Yes	Yes	Yes	Yes	X	X	X	X
5	Yes	No	No	No	No	X	X	X
6	Yes	No	No	No	Yes	Yes	X	X
Pre-Phase	Yes	Yes	Yes	Yes	Yes	Yes	Yes	X

Yes = Maintenance of Traffic Phases may be constructed concurrently
No = Maintenance of Traffic Phases shall not be constructed concurrently

Legend

-  Pre-Phase - No Traffic Impacts
-  Phase 1 - No Traffic Impacts
-  Phase 2A - ODOT SCD MT-97.10
-  Phase 2B - ODOT SCD MT-97.10
-  Phase 3 - See Sheet 14
-  Phase 4 - See Sheet 15
-  Phase 5 - See Sheets 16-17
-  Phase 6 - ODOT SCD MT-95.61



MAINTENANCE OF TRAFFIC ESTIMATE OF QUANTITIES

Item No.	Estimated Quantity	Unit	Item Description
614	1	Lump Sum	Maintaining Traffic, As Per Plan

GRAPHIC SCALE: 1 inch = 100 feet

CALCULATED: BDM
CHECKED: JDS

MAINTENANCE OF TRAFFIC NOTES

CHESHIRE ROAD RELOCATION

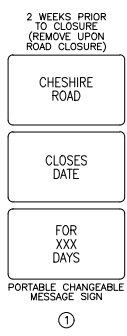
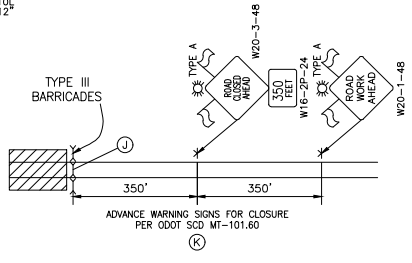
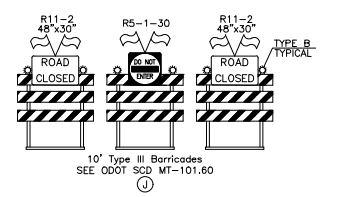
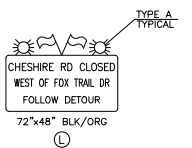
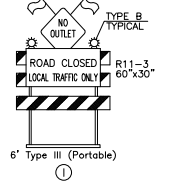
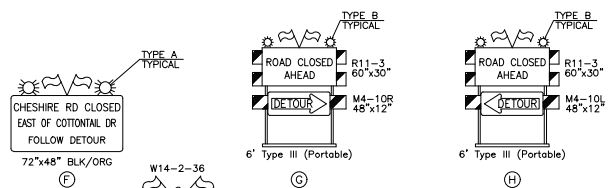
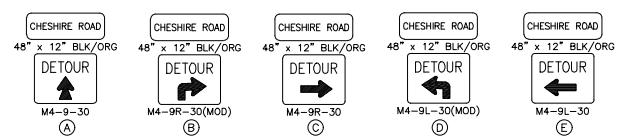
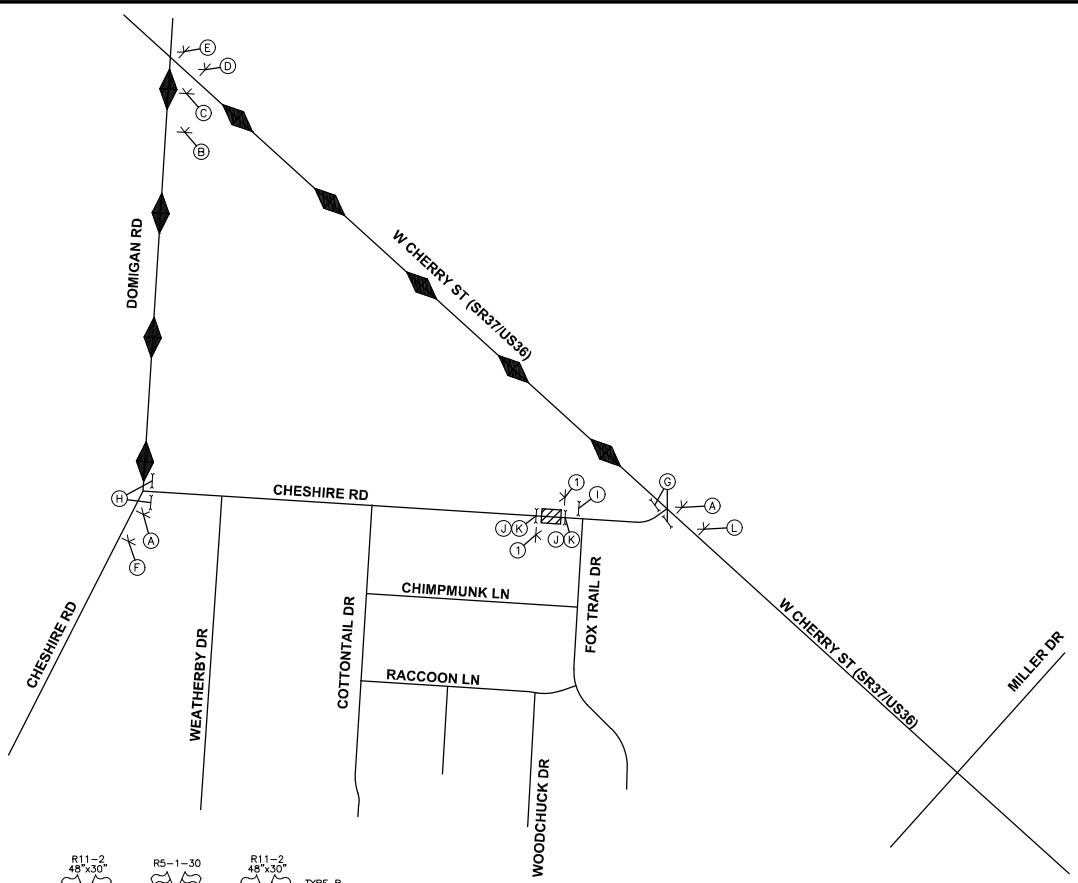


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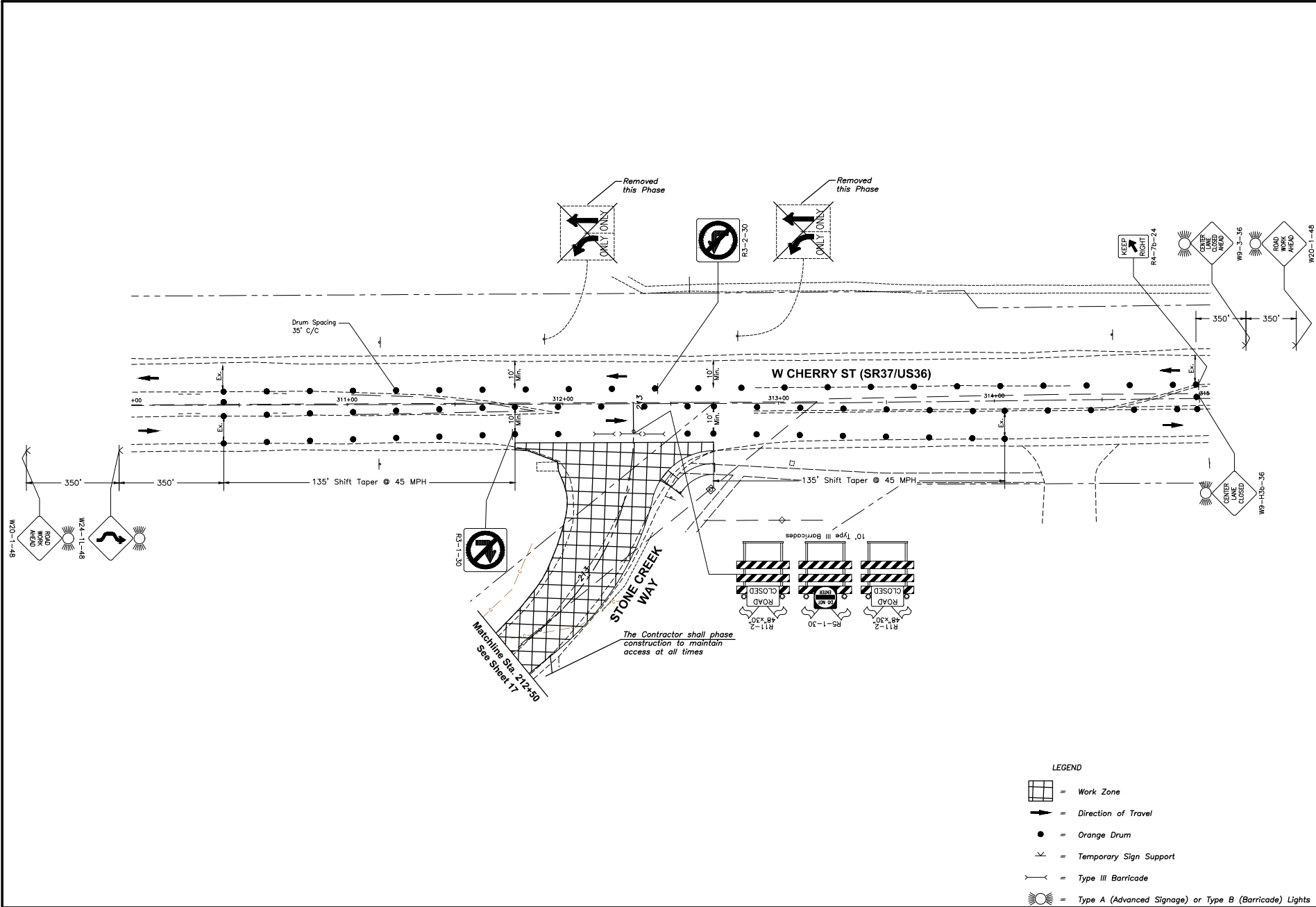
MAINTENANCE OF TRAFFIC PLAN
PHASE 4 DETOUR

CHESHIRE ROAD RELOCATION



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LEGEND

- = Work Zone
- = Direction of Travel
- = Orange Drum
- = Temporary Sign Support
- = Type III Barricade
- = Type A (Advanced Signage) or Type B (Barricade) Lights

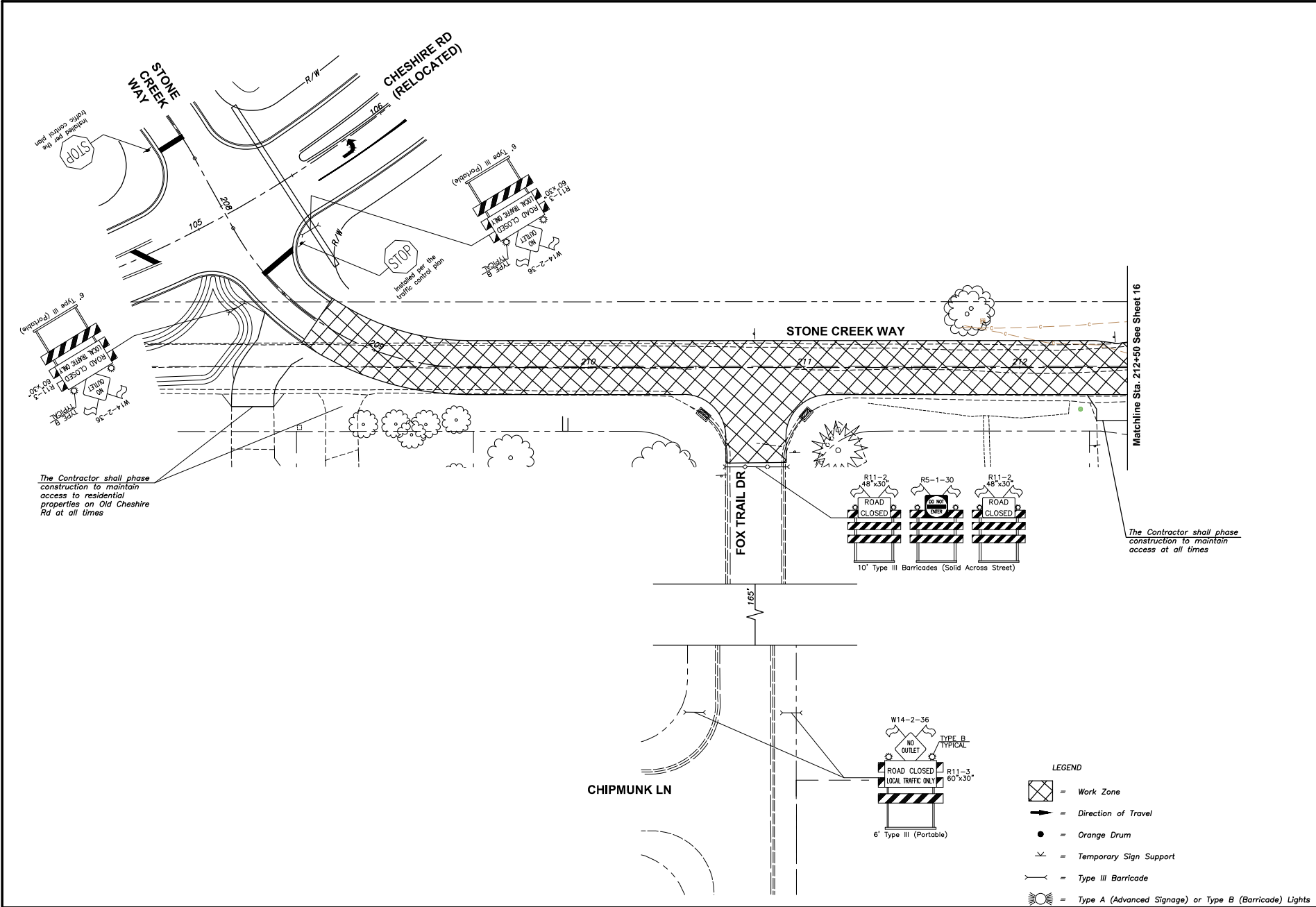
CHESHIRE ROAD RELOCATION
MAINTENANCE OF TRAFFIC PLAN
PHASE 5

CALCULATED BDM	CHECKED JDS
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GRAPHIC SCALE:
 0 10 20 30 40 50
 1 inch = 30 feet

16
 65

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The Contractor shall phase construction to maintain access to residential properties on Old Cheshire Rd at all times

The Contractor shall phase construction to maintain access at all times

- LEGEND**
- = Work Zone
 - = Direction of Travel
 - = Orange Drum
 - = Temporary Sign Support
 - = Type III Barricade
 - = Type A (Advanced Signage) or Type B (Barricade) Lights

GRAPHIC SCALE

1 inch = 30 feet

CALCULATED	BDM	CHECKED	JDS
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MAINTENANCE OF TRAFFIC PLAN

PHASE 5

CHESHIRE ROAD RELOCATION

PHASE 5

17

65

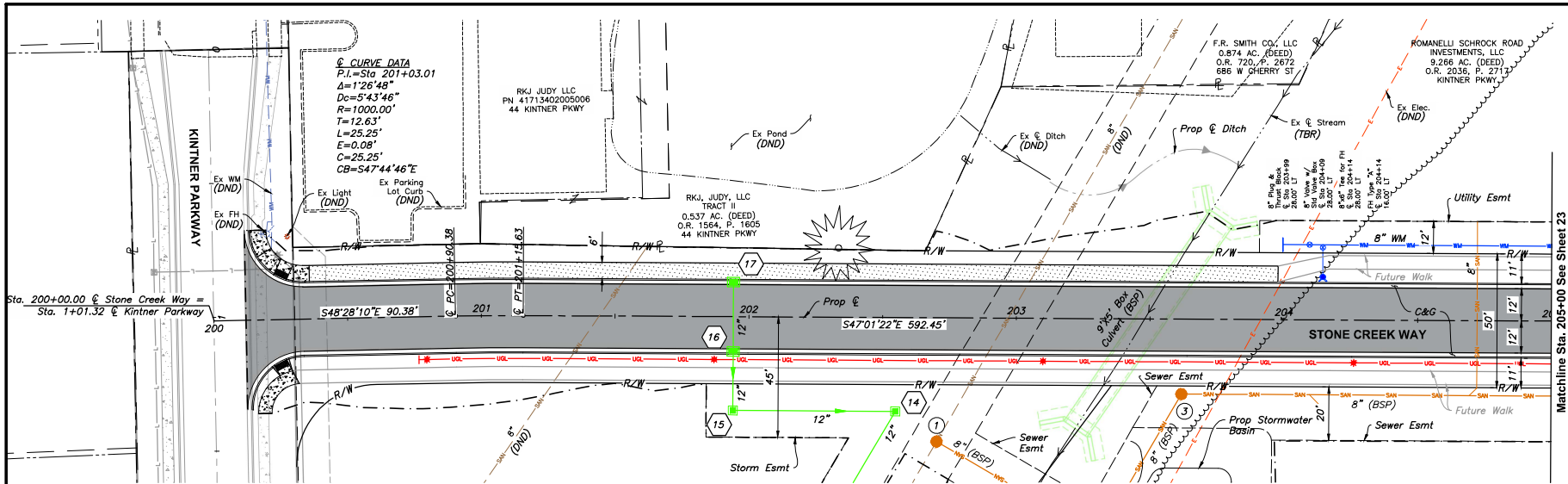
ITEM NO.	ESTIMATED QUANTITY	UNIT	DESCRIPTION	SEE SHEET
Roadway				
201	1	L. Sum	Clearing and Grubbing	
202	1	Each	Manhole Removed	
202	113	Ft	Pipe Removed, 24" and Under	
202	489	Sq Ft	Concrete Walk Removed	
202	2	Each	C&G Inlet Removed	
202	27	Sq Yd	Pavement Removed (Concrete Drive)	
202	59	Ft	Curb and Gutter Removed	
202	24	Ft	Removal Misc.: Timber Driveway Edge Removed and Reset	
202	682	Ft	Guardrail Removed	
203	7,804	Cu Yd	Excavation	
203	4,075	Cu Yd	Embankment	
204	6,735	Sq Yd	Subgrade Compaction	
204	4	Hour	Proof Rolling	7
204*	1,200	Cu Yd	Excavation of Subgrade	7
204*	1,200	Cu Yd	Granular Material, Type B	7
204*	1,690	Sq Yd	Geotextile Fabric, Type D	7
608	2,214	Sq Ft	Concrete Walk (T=4")	
608	1,084	Sq Ft	Concrete Walk (T=8")	
608	8	Each	Curb Ramps	
608	120	Sq Ft	Detectable Warning	
Special	5	Each	Mulch/Box Removed and Reset	7
Sediment & Erosion Control				
207*	7,700	Sq Yd	Construction Seeding and Mulching	
207	1,840	Ft	Filter Sock	11
207	216	Ft	Compost Sack Check Dam	11
207	30	Each	Inlet Protection	11
207	1	Each	Designated Concrete Washout Area	11
207	1	L. Sum	Stabilizer Construction Entrance	11
659	1,283	Ton	Topsoil (T=7)	
659	2.1	55N	Commercial Fertilizer	
659	15,392	Sq Yd	Seeding and Mulching, Class 1	
659	328	Sq Yd	Native Wildflower and Grass Mixture	
659	84	M Gal	Water	
Special	1	Each	Temporary Sediment Basin Dewatering Skimmer	44
Special	1,187	Sq Yd	Permanent Erosion Control Matting	7
Drainage				
601	15	Cu Yd	Rock Channel Protection, Type C with Fabric Filter	
603	300	Ft	4" Conduit, Type F	
604	8	Each	Catch Basin 2x2 (DCED-S133A)	
604	1	Each	Catch Basin 3x3 (DCED-S133C)	
604	2	Each	Catch Basin 4x4 (DCED-S133D)	
604	12	Each	Curb and Gutter Inlet Single (DCED-S125)	
604	2	Each	Manhole, Type C (DCED-S102)	
604	5	Each	Manhole, Manhole No.3 (ODOT MH-3, 90" Base, One W/ Flat Top Transition See Plan)	
604	1	Each	Headwall for 18" Pipe	
604	1	Each	Headwall for 24" Pipe	
604	1	Each	Headwall for 60" Pipe	
605	3,742	Ft	4" Underdrain	
000T 611*	100	Ft	4" Conduit, Type E	7
000T 611*	100	Ft	6" Conduit, Type E	7
000T 611*	100	Ft	8" Conduit, Type E	7
000T 611*	100	Ft	12" Conduit, Type E	7
901	652	Ln Ft	12" Pipe w/ Type 1 Bedding	
901	81	Ln Ft	12" Pipe w/ Type 1 Bedding (w/ Watertight Joints and Concrete Encasement per DCED-S149 & CMS 910)	
901	49	Ln Ft	15" Pipe w/ Type 1 Bedding (w/ Watertight Joints)	
901	71	Ln Ft	15" Pipe w/ Type 1 Bedding (w/ Watertight Joints and Concrete Encasement per DCED-S149 & CMS 910)	
901	26	Ln Ft	18" Pipe w/ Type 1 Bedding	
901	290	Ln Ft	24" Pipe w/ Type 1 Bedding (w/ Watertight Joints)	
901	639	Ln Ft	30" Pipe w/ Type 1 Bedding	
Special	2	Ln Ft	34" Elliptical Pipe	
Special	596	Cu Yd	Clay Liner	7
Special	1	Each	Detention Outlet Structure	7
Pavement				
252	12	Sq Yd	Pavement Repair	
301	1,008	Cu Yd	Asphalt Concrete Base, PG64-22 (T=6")	
304	12	Cu Yd	Aggregate base (T=6") (Drives)	
304	1,008	Cu Yd	Aggregate Base (T=6")	
407	4	Gal	Non-Tracking Tack Coat (0.055 Gal/SY) (Drives)	
407	666	Gal	Non-Tracking Tack Coat (0.055 Gal/SY)	
423	178	Pound	Crack Sealing	
441	3	Cu Yd	Asphalt Concrete, Surface Course, Type 1, (448), PG64-22 (T=1.25") (Drives)	
441	4	Cu Yd	Asphalt Concrete, Intermediate Course, Type 2, (448), PG64-22 (T=1.25") (Drives)	
441	297	Cu Yd	Asphalt Concrete, Intermediate Course, Type 2, (448), PG64-22 (T=1.25")	
441	212	Cu Yd	Asphalt Concrete, Surface Course, Type 1, (448), PG64-22 (T=1.25")	
452	231	Sq Yd	Non-Reinforced Concrete Pavement (T=8")	
609	3,742	Ft	Combination Curb and Gutter (DCED-R2020)	
609	126	Sq Yd	9" Concrete Median (Cals Std Dwg 2331)	
Lighting				
1001	7	Each	Pull Box, Medium Duty	
1001	1	Each	Riser, Street Light Circuit	
1001	1,819	CHT Ft	Street Lighting Circuit Cable, As Per Plan	61
1001	14	Each	Pole to be Wired, As Per Plan	61
1001	1	Each	120/240V Pad Mounted Lighting Controller, As Per Plan	61
1001	1,799	Ln Ft	2" Conduit, Concrete Encased	61
1001	7	Each	Luminaire, 59W, 240V, With House Side Shield, As Per Plan	61
1001	1	Each	Luminaire, 59W, 240V, With House Side Shield, As Per Plan, Furnish Only	61
1001	7	Each	Luminaire, 77W, 240V, With House Side Shield, As Per Plan	61
1001	1	Each	Luminaire, 77W, 240V, With House Side Shield, As Per Plan, Furnish Only	61
1001	14	Each	Light Pole, As Per Plan	61
1001	1	Each	Light Pole, As Per Plan, Furnish Only	61
1001	14	Each	Foundation, As Per Plan	61

ITEM NO.	ESTIMATED QUANTITY	UNIT	DESCRIPTION	SEE SHEET
Traffic Control				
621	18	Each	RPM	47
621	4	Each	Rollup Pavement Marker Removed	47
621	8	Each	RPM, Misc.: RPM Reflector Removed and Replaced	47
627	25	Each	Reboundable Traffic Post - Yellow Installed	47
630	378	Ft	Ground Mounted Support, No. 3 Post	
630	184	Sq Ft	Sign, Flat Sheet	
630	1	Each	Removal of Ground Mounted Sign and Reerection	
630	9	Each	Removal of Ground Mounted Sign and Storage	
630	7	Each	Removal of Ground Mounted Post Support and Disposal	
630	6	Each	Sign, Double-Faced, Street Name	
644	6	Each	Removal of Pavement Marking	
644	2300	Ft	Removal of Pavement Marking	
644	0.05	Mile	Center Line, Solid and Dashed, 4"	
644	0.51	Mile	Center Line, Double Yellow, 4"	
644	0.10	Mile	Edge Line, Yellow 8"	
644	851	Ft	Channelizing Line, 8"	
644	102	Ft	Crosswalk Line, 12"	
644	201	Ft	Stop Line, 24"	
644	193	Ft	Transverse/Diagonal Line, Yellow, 24"	
644	44	Ft	Chevron Marking, 24"	
644	13	Each	Lane Arrow, 96"	
647	480	Ft	Crosswalk Line, Type II, 24", Type B90	
Traffic Signals				
000T 625	120	Ln Ft	Conduit Encased, 2", 725.051	
000T 625	290	Ln Ft	Conduit Encased, 3", 725.051	
000T 625	206	Ln Ft	Conduit, 2", 725.051	
000T 625	324	Ln Ft	Conduit, 3", 725.051	
000T 625	418	Ln Ft	Conduit, 3", 725.052 (SCH 80), Jacketed or Drilled, As Per Plan	51
000T 625	185	Each	Pull Box, 225.08, 24", As Per Plan	51
000T 625	4	Each	Pull Box, 225.08, 24", As Per Plan	51
000T 625	418	Ln Ft	Trench	
000T 625	418	Ln Ft	Plastic Caution Tape	
000T 625	10	Each	Ground Rod	
000T 625	4	Each	Luminaire, Post Top, LED, 120 Volt, As Per Plan	51
000T 625	4	Each	Connection, Fused Pull-Apart, Type II	
000T 625	4	Each	Connection, Unfused Pull-Apart, Type III	
000T 625	1,316	Ln Ft	No. 8 AWG, 600 Volt Distribution Cable (Combination Lighting)	
000T 625	707	Ln Ft	No. 4 AWG, 600 Volt Distribution Cable, As Per Plan (Grounding and Bonding)	51
000T 625	300	Ln Ft	Pole and Bracket Cable, No. 10 AWG, 600 Volt	
000T 630	4	Each	Sign, Single Faced, Street Name, As Per Plan	51
000T 630	1	Each	Sign, Misc.: Traffic Signal Signs	51
000T 632	9	Each	Vehicular Signal Head, (LED), 3-Section, 12" Lens, 1-Way, With Backplate (Black), As Per Plan	52
000T 632	2	Each	Vehicular Signal Head, (LED), 4-Section, 12" Lens, 1-Way, With Backplate (Black), As Per Plan	52
000T 632	8	Each	Pedestrian Signal Head, As Per Plan	52
000T 632	11	Each	Covering of Vehicular Signal Head, As Per Plan	52
000T 632	8	Each	Covering of Pedestrian Signal Head	
000T 632	8	Each	Accessible Pedestrian Pushbutton, As Per Plan	52
000T 632	1	Each	Combination Signal Support, Type TC-81.22, Design No. 12, As Per Plan	52
000T 632	1	Each	Combination Signal Support, Type TC-81.22, Design No. 13, As Per Plan	52
000T 632	2	Each	Combination Signal Support, Type TC-81.22, Design No. 14, As Per Plan	52
000T 632	4	Each	Signal Support Foundation, As Per Plan	52
000T 632	4	Each	Pedestrian Pedestal, 11", As Per Plan	52
000T 632	2,654	Ln Ft	Signal Cable, 7 Conductor, No. 14 AWG	
000T 632	1,834	Ln Ft	Signal Cable, 3 Conductor, No. 14 AWG	
000T 632	1,618	Ln Ft	Loop Detector Lead-in Cable	
000T 632	1	Each	Conduit Riser, 2" Diameter, SCH 80	
000T 632	158	Ln Ft	Power Cable, 3-Conductor, Cu, No. 6 AWG, As Per Plan	52
000T 632	1	Each	Power Service, As Per Plan	52
000T 633	1	Each	Cabinet, Type TS-2, As Per Plan	52
000T 633	1	Each	Cabinet Foundation, As Per Plan	53
000T 633	1	Each	Controller Work Pad, As Per Plan	53
000T 633	1	Each	Uninterruptible Power Supply (UPS), 1000 Watt, As Per Plan	53
000T 633	1	Each	Communication System, As Per Plan	53
000T 633	1	Each	Controller Item, Misc: GPS Time Synchronization	53
000T 809	1	Each	ATC Controller, As Per Plan	52
000T 809	1	Each	CCTV IP-Camera System, Dome-Type	53
000T 816	1	Each	Video Detection System, As Per Plan	53
Special	4	Each	Pole Mounted Receptacle, 120V, Duplex GFI	53
Maintenance of Traffic				
614	1	L. Sum	Maintaining Traffic, As Per Plan	13
616	100	M Gal	Water	7
Water				
801	39	Ln Ft	6" Water Pipe and Fittings	
801	1,065	Ln Ft	6" Water Pipe and Fittings	
802	3	Each	6" Valve & Appurtenances	
802	8	Each	6" Valve & Appurtenances	
806	225	Ln Ft	6" Casing Pipe	
809	3	Each	Fire Hydrant, Type "B"	
Sanitary				
604	5	Each	Manhole, Type C (So.5-3)	
604	1	Each	Doghouse Manhole per DCRSD Std Dwg So.5-4	
901	648	Ln Ft	6" Sanitary Sewer w/ Type 1 Bedding	
914	4	Each	6"x6" Diameter Wye Fitting	
Miscellaneous				
623	1	L. Sum	Construction Layout Stakes	
624	1	L. Sum	Mobilization	

Notes:
 Item numbers reference current Columbus CMS unless otherwise noted.
 000T = Ohio Department of Transportation Construction & Material Specifications, 2023 Edition
 * = To be used at the direction of the Engineer

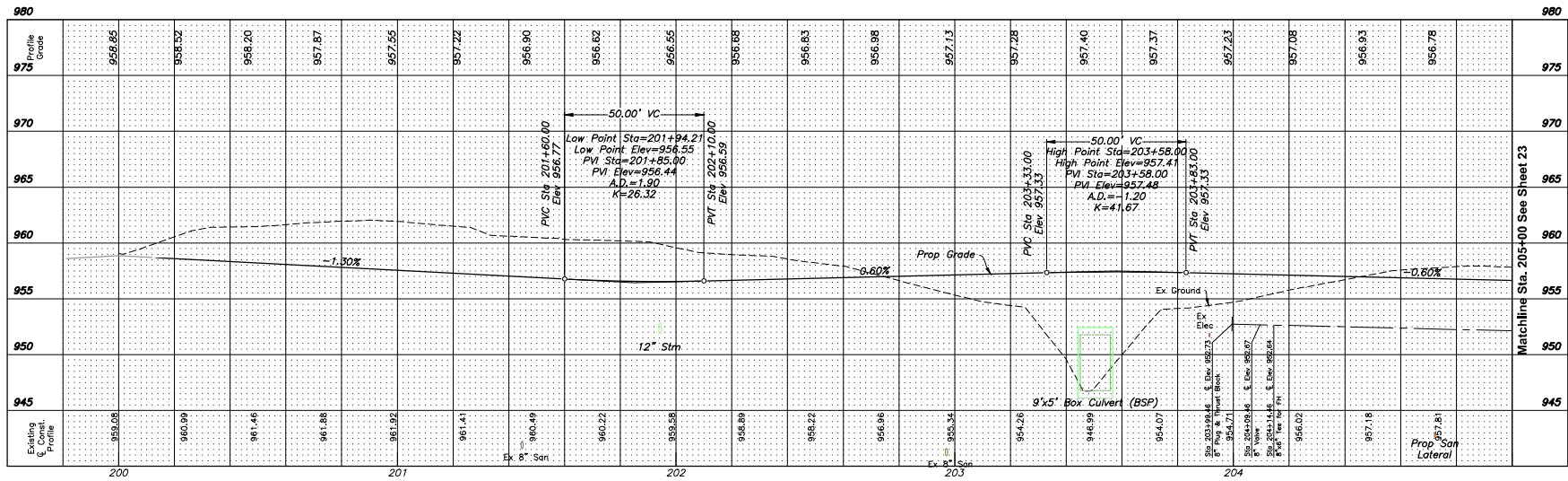
CALCULATED
 JJB
 CHECKED
 MJS
 GENERAL SUMMARY
 CHESHIRE ROAD RELOCATION
 18
 65

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Legend

- 4" Concrete Walk
- Full Depth Pavement
- 8" Concrete Walk
- Concrete Drive
- Detectable Warning
- Const Limits



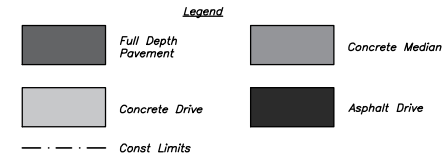
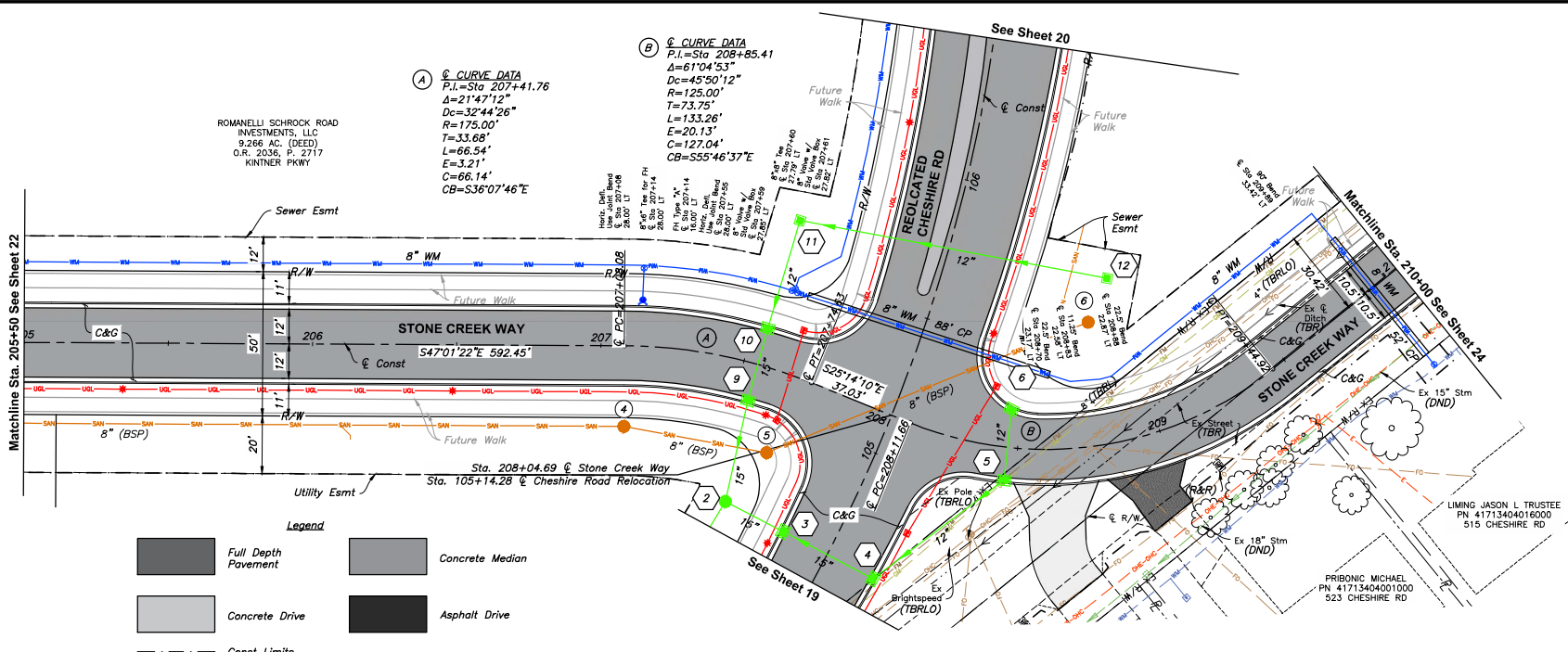
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CHECKED	JOB	DATE
VERIFIED	NO.	DATE
APPROVED	BY	DATE
MUS		

PLAN & PROFILE

CHESHIRE ROAD RELOCATION

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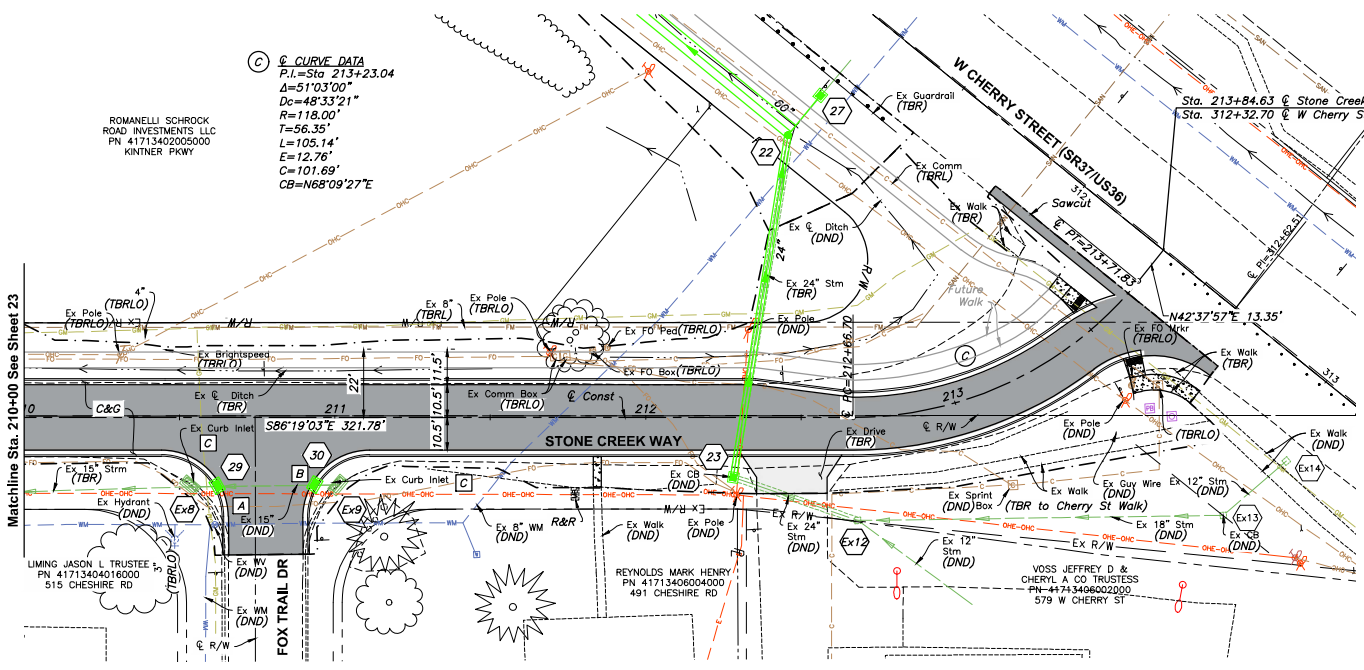


Station	Profile Grade	Prop. Grade	Notes
975			
970	955.88		
965	956.33		
960	956.18		
955	956.03	0.60%	Ex Ground
950	955.88		
945	955.73		
940	955.58		
935	955.43		
930	955.28		
925	955.19		
920	955.23		
915	955.37		
910	955.28		
905	955.23		
900	955.32		
895	955.55		
890	955.85		
885	956.16		
880	956.46		
875			

PLAN & PROFILE
CHESHIRE ROAD RELOCATION
 23
 65

© CURVE DATA
 P.I.=Sta. 213+23.04
 $\Delta=51^{\circ}03'00''$
 $D_c=48^{\circ}33'21''$
 $R=118.00'$
 $T=56.35'$
 $L=105.74'$
 $E=12.76'$
 $C=101.69'$
 $CB=N68^{\circ}09'27''E$

ROMANELLI SCHRÖCK
 ROAD INVESTMENTS LLC
 PN 41713402005000
 KINTNER PKWY

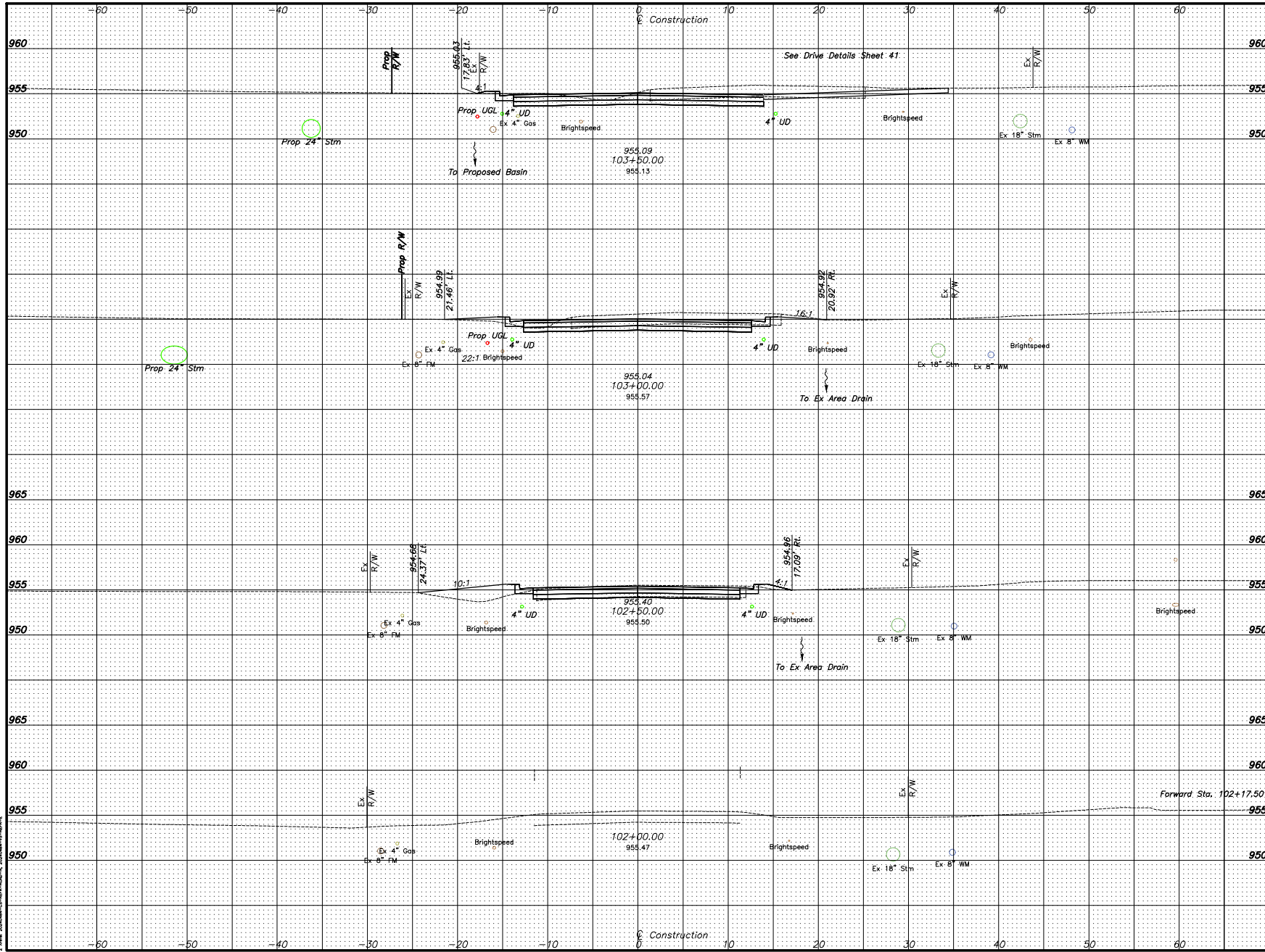


- Legend**
- Full Depth Pavement
 - Concrete Drive
 - 8" Concrete Walk
 - Const Limits
 - Detectable Warning
- A** DCED-S125
Stm. Curb Inlet
TC=958.50
15" W=955.24
UD= 956.54
Sta. 210+62.17
Offset: 21.96' Rt.
- B** DCED-S125
Stm. Curb Inlet
TC=957.97
15" W=955.00
UD= 956.01
Sta. 210+93.51
Offset: 21.55' Rt
- C** TBA Connect pipes through

Profile	945	950	955	960	965	970	975
Existing & Const. Profile	945.22	945.44	945.74	945.85	946.20	946.56	946.93
Profile Grade	957.07	957.37	957.67	957.99	958.28	958.58	958.89
Notes		1.21%		Ex Ground	Prop Grade		
Vertical Curve Data					50.00' VC PVI Sta=212+00.00 PVI Elev=959.19 A1=1.24 K=40.41 PVC Sta 211+75.00 ELEV 958.89 PVT Sta 212+25.00 ELEV 959.80		
Other Notes							2.45%

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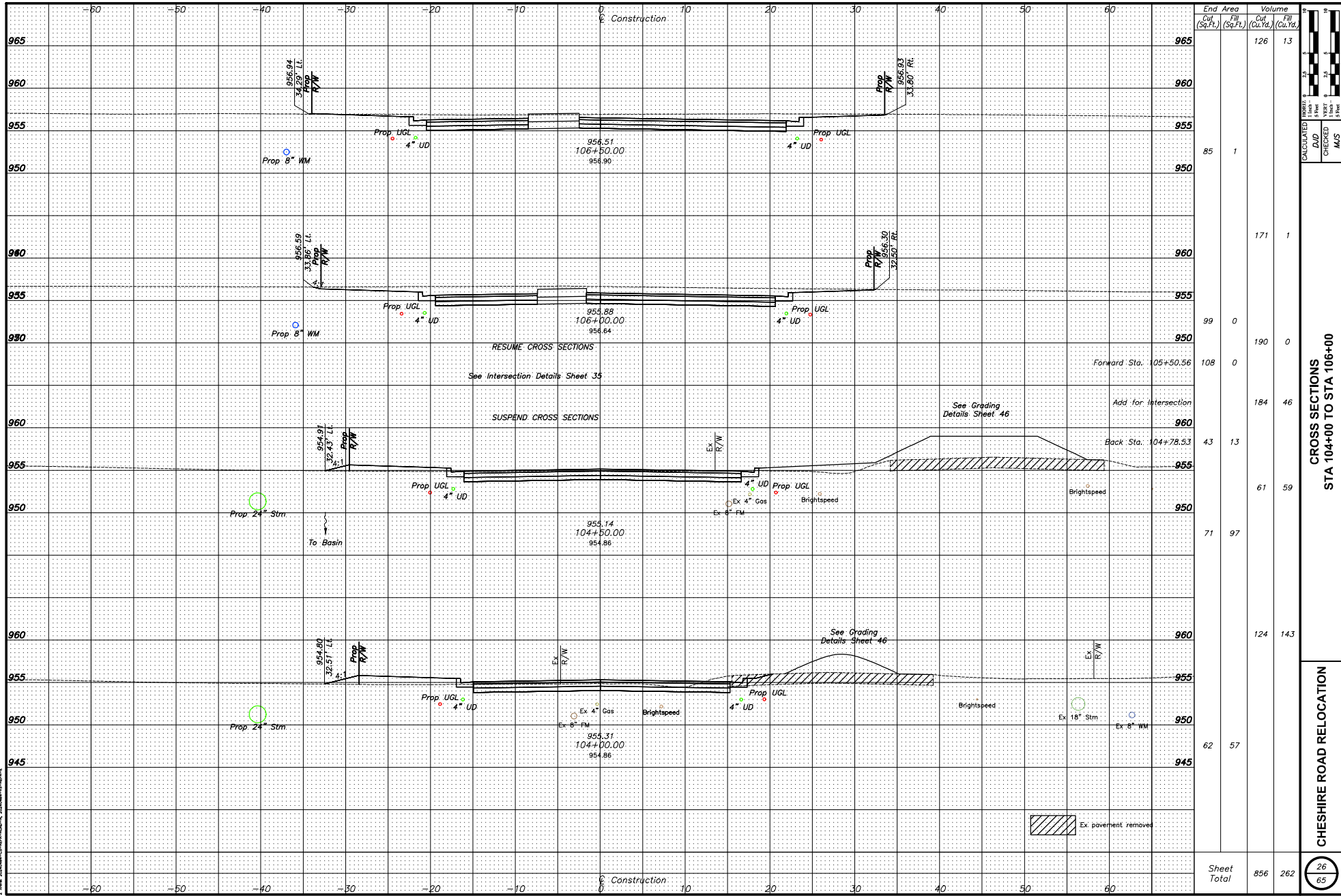


Station	End Area		Volume	
	Cut (Sq.Ft.)	Fill (Sq.Ft.)	Cut (Cu.Yd.)	Fill (Cu.Yd.)
960			94	14
955				
950	65	2		
			105	6
	48	4		
965			75	17
960				
955	33	14		
950				
965			40	9
960				
955	33	0		
950				
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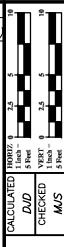
CROSS SECTIONS
STA 102+00 TO STA 103+50
CHESHIRE ROAD RELOCATION

CALCULATED
 DUD
 CHECKED
 MAS

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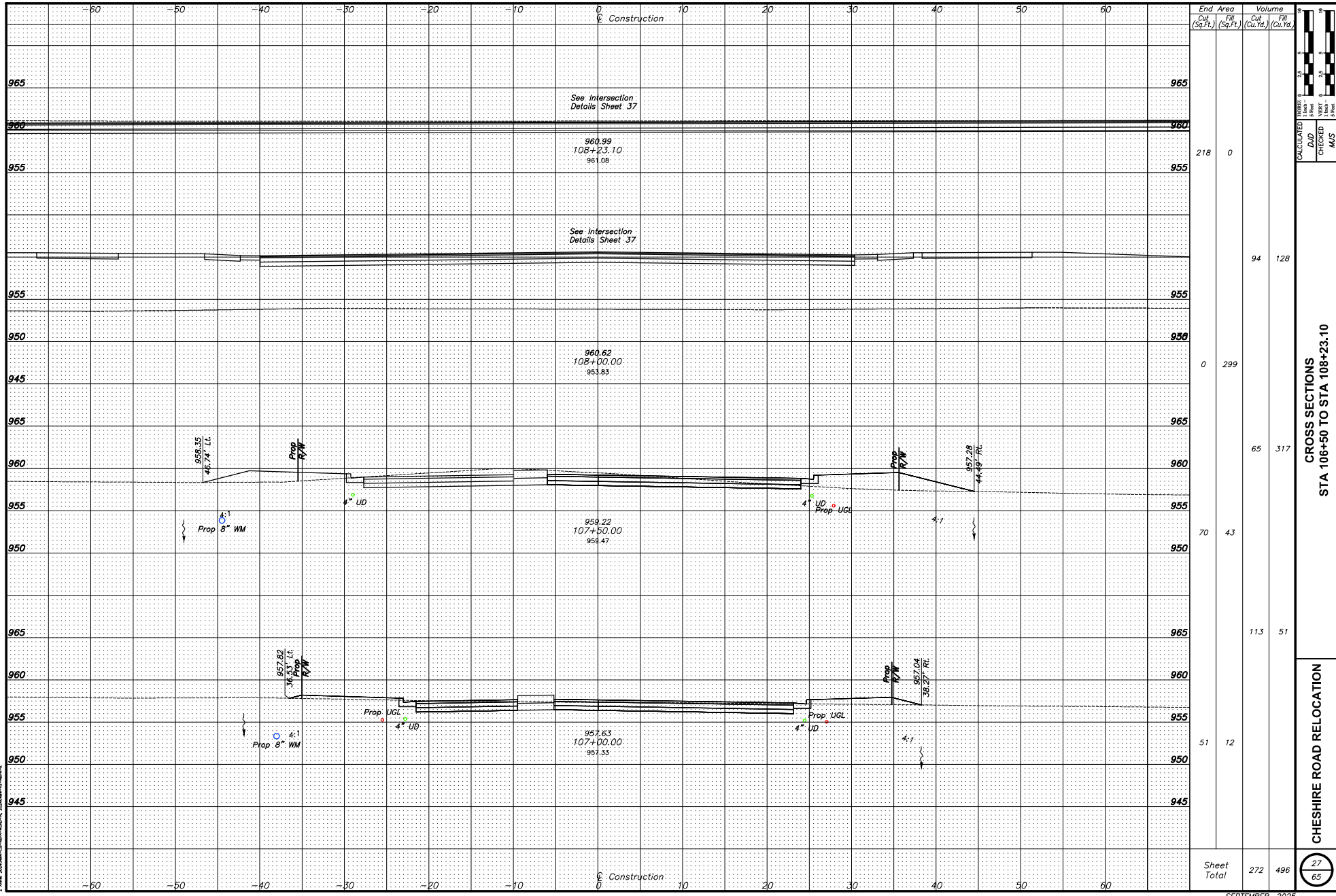
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965			126	13
960				
955				
950	85	1		
960			171	1
955				
950	99	0		
960			190	0
955				
950	108	0		
960			184	46
955	43	13		
950	71	97		
960			124	143
955				
950	62	57		
945				
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CROSS SECTIONS
STA 104+00 TO STA 106+00

CHESHIRE ROAD RELOCATION

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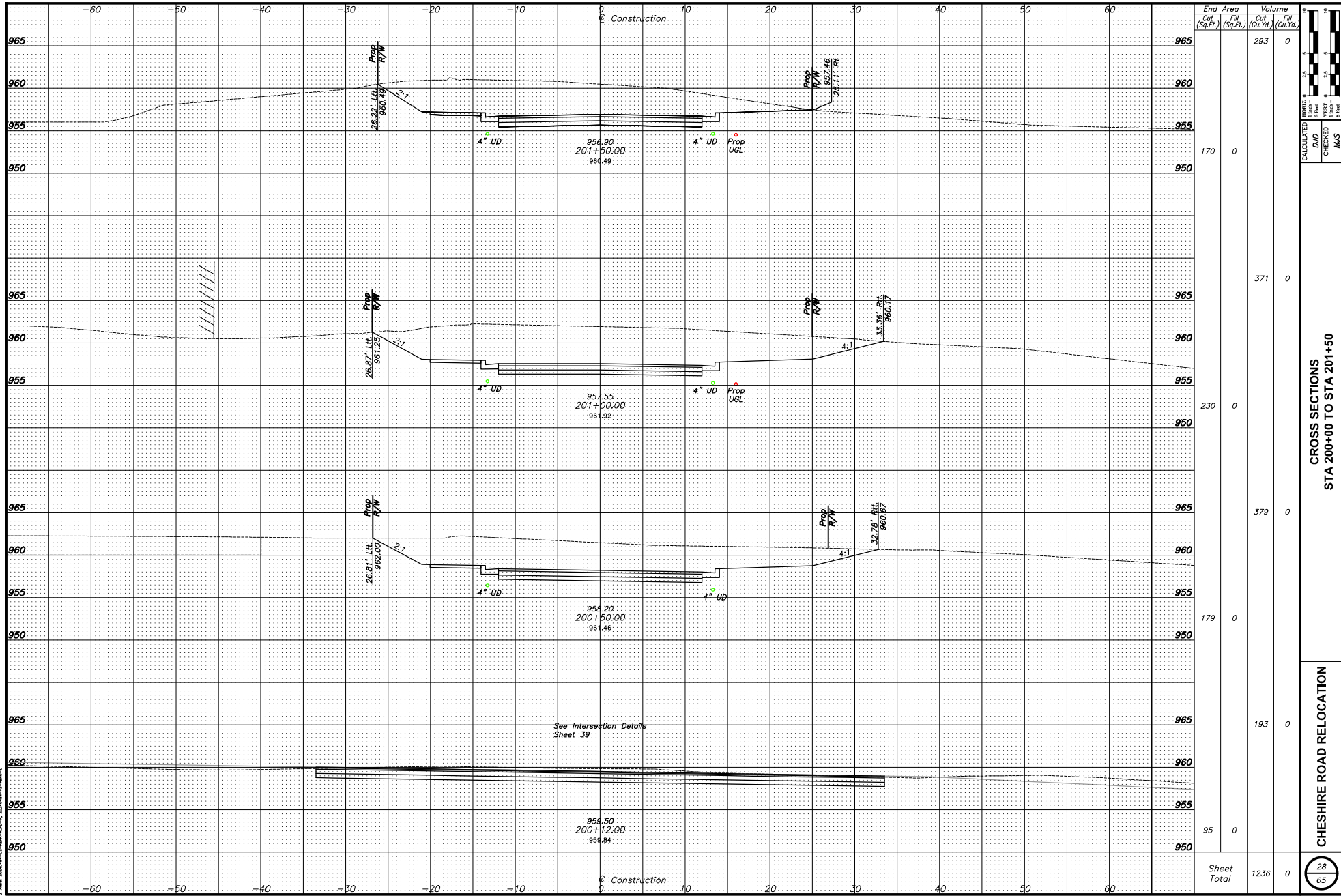
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965				
960				
955	218	0		
955			94	128
950				
945	0	299		
965				
960			65	317
955				
950			70	43
965				
960			113	51
955				
950			51	12
945				
Sheet Total			272	496

CROSS SECTIONS
STA 106+50 TO STA 108+23.10

CALCULATED	DJD
CHECKED	MJS

27
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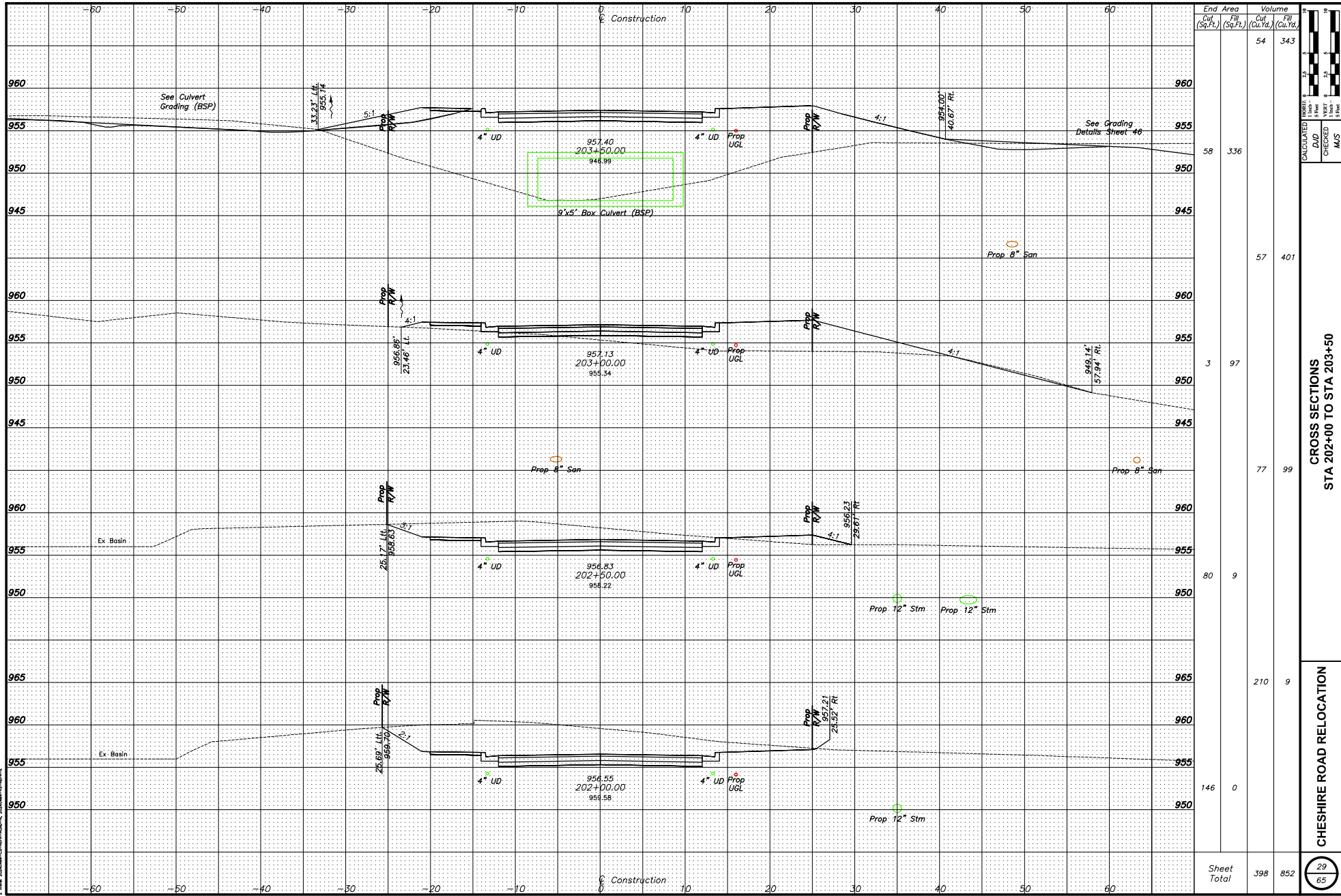
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960				
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950	170	0		
965			371	0
960				
955				
950	230	0		
965			379	0
960				
955				
950	179	0		
965			193	0
960				
955				
950	95	0		
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 CHECKED BY: *[Signature]*
 DATE: *[Date]*

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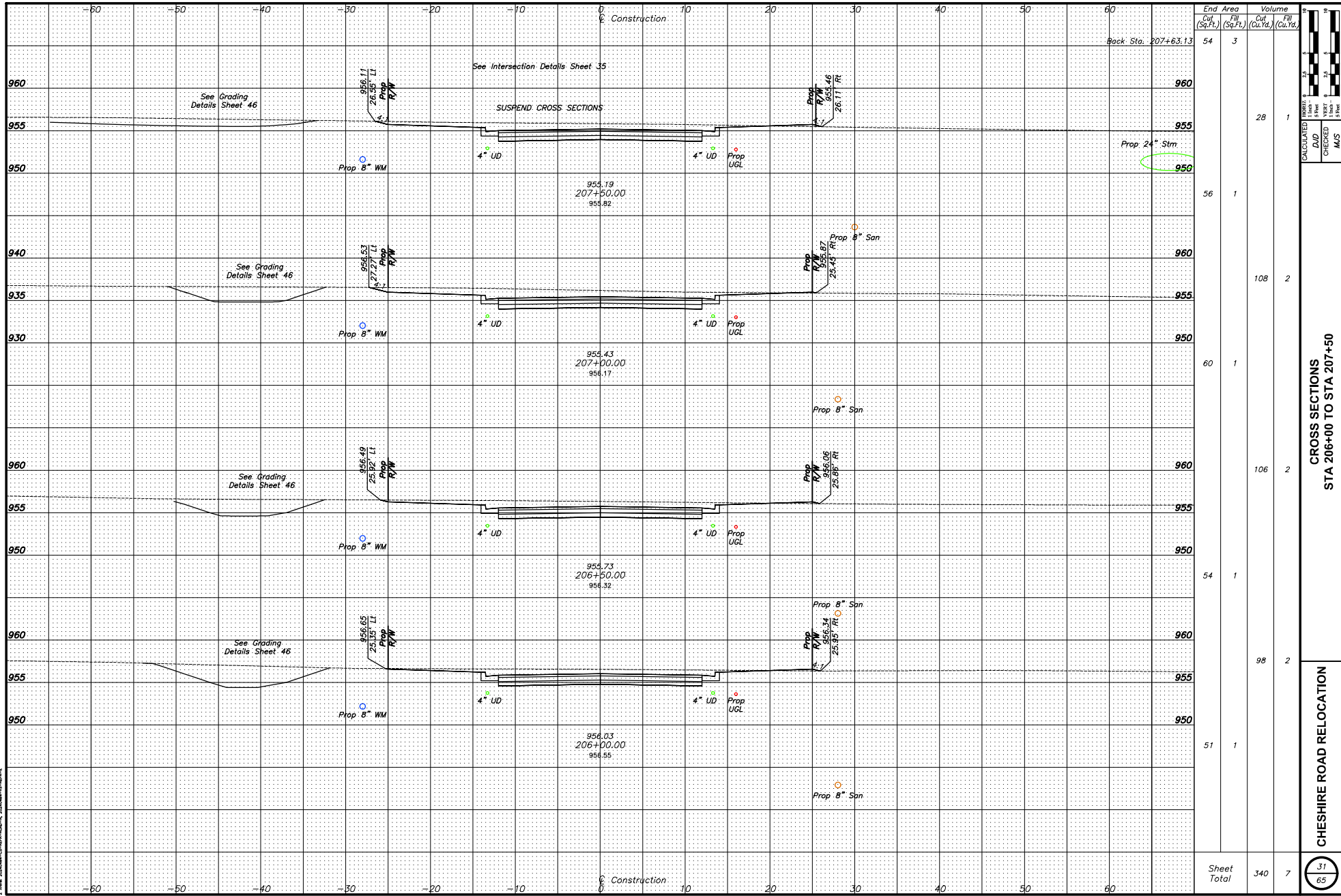
Station	End Area		Volume	
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955			58	3.36
950			57	401
945			3	97
960			77	99
955			80	9
950			146	0
965			210	9
960				
955				
950				
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 CHECKED: [Signature]
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CHESHIRE ROAD RELOCATION

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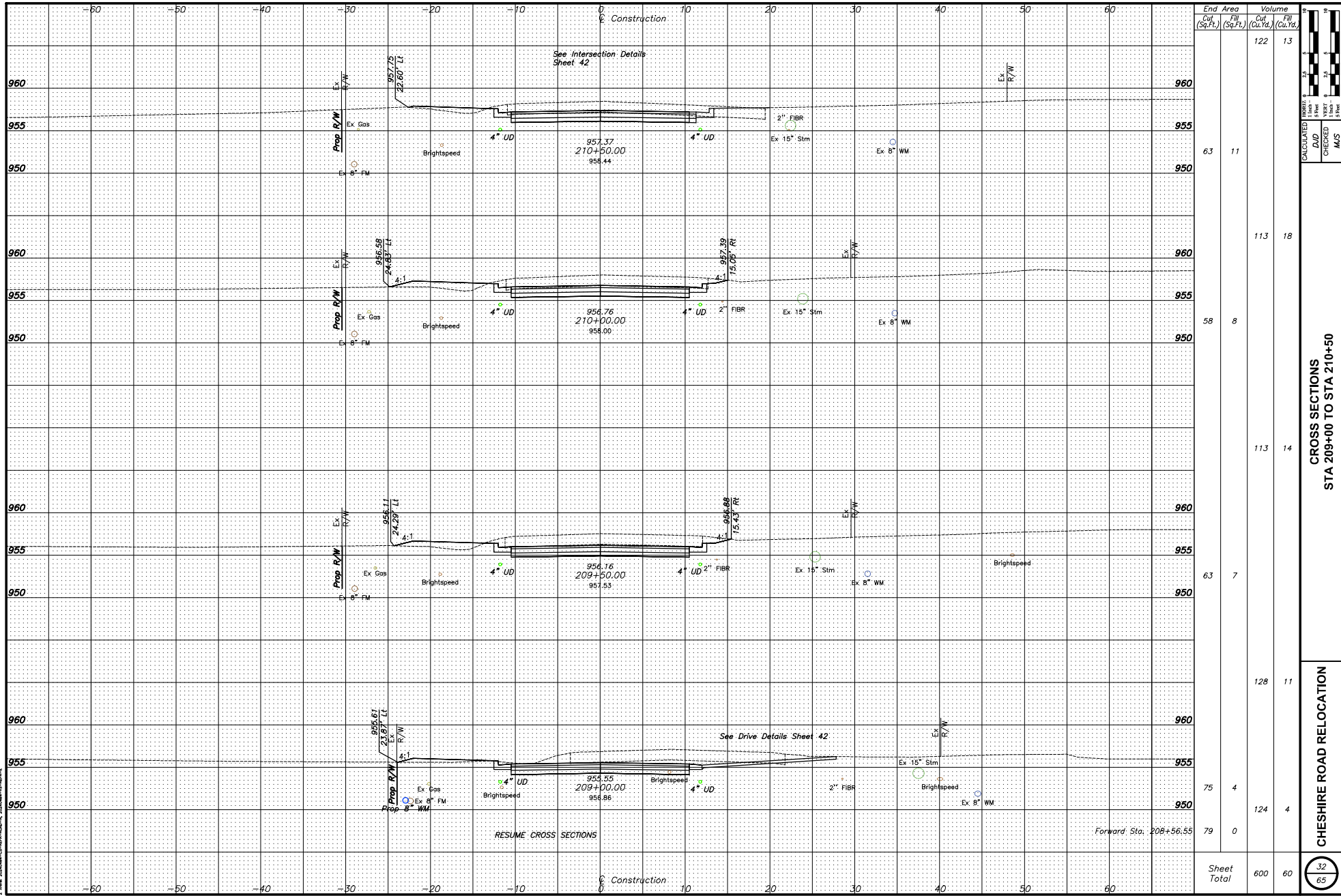
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		Cut (Sq.Ft.)	Fill (Sq.Ft.)	Cut (Cu.Yd.)	Fill (Cu.Yd.)
207+63.13	960	54	3		
207+50.00	955	56	1		
207+00.00	950	60	1		
206+50.00	955	54	1		
206+00.00	950	51	1		
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CALCULATED	REVISION	DATE
DVD		
CHECKED		
MAS		

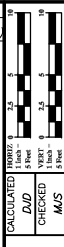
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CHESHIRE ROAD RELOCATION

31
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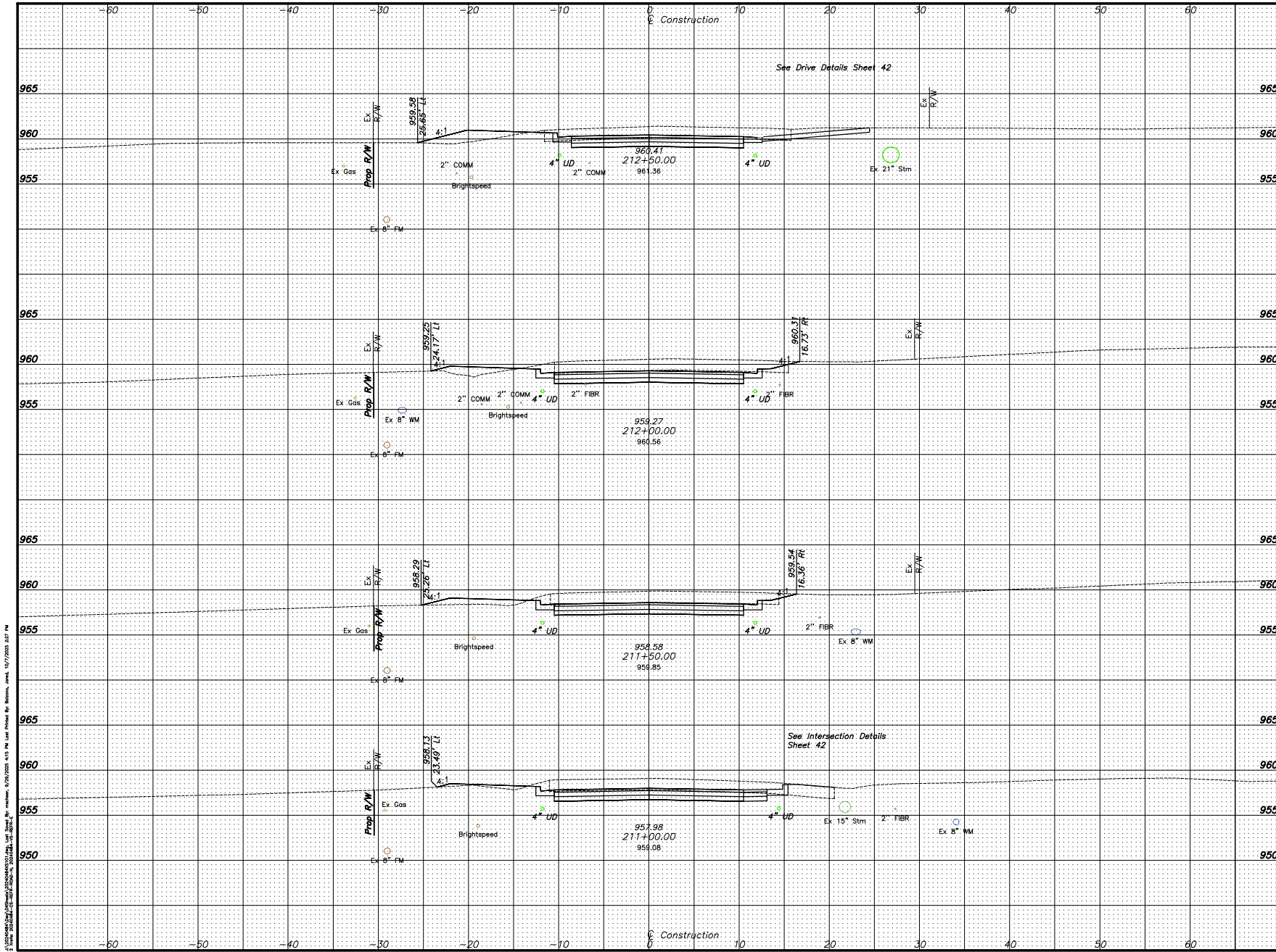
Station	End Area		Volume	
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955	63	11		
950				
960			113	18
955	58	8		
950				
960			113	14
955	63	7		
950				
960			128	11
955	75	4		
950			124	4
Forward Sta. 208+56.55	79	0		
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CROSS SECTIONS
 STA 209+00 TO STA 210+50

CHESHIRE ROAD RELOCATION

32
 65



Station	End Area		Volume	
	Cut (Sq.Ft.)	Fill (Sq.Ft.)	Cut (Cu.Yd.)	Fill (Cu.Yd.)
965			103	29
960				
955	61	10		
965			118	15
960				
955	66	6		
965			120	13
960				
955	63	7		
965				
960			122	10
955				
950	68	3		
Sheet Total	463	67		

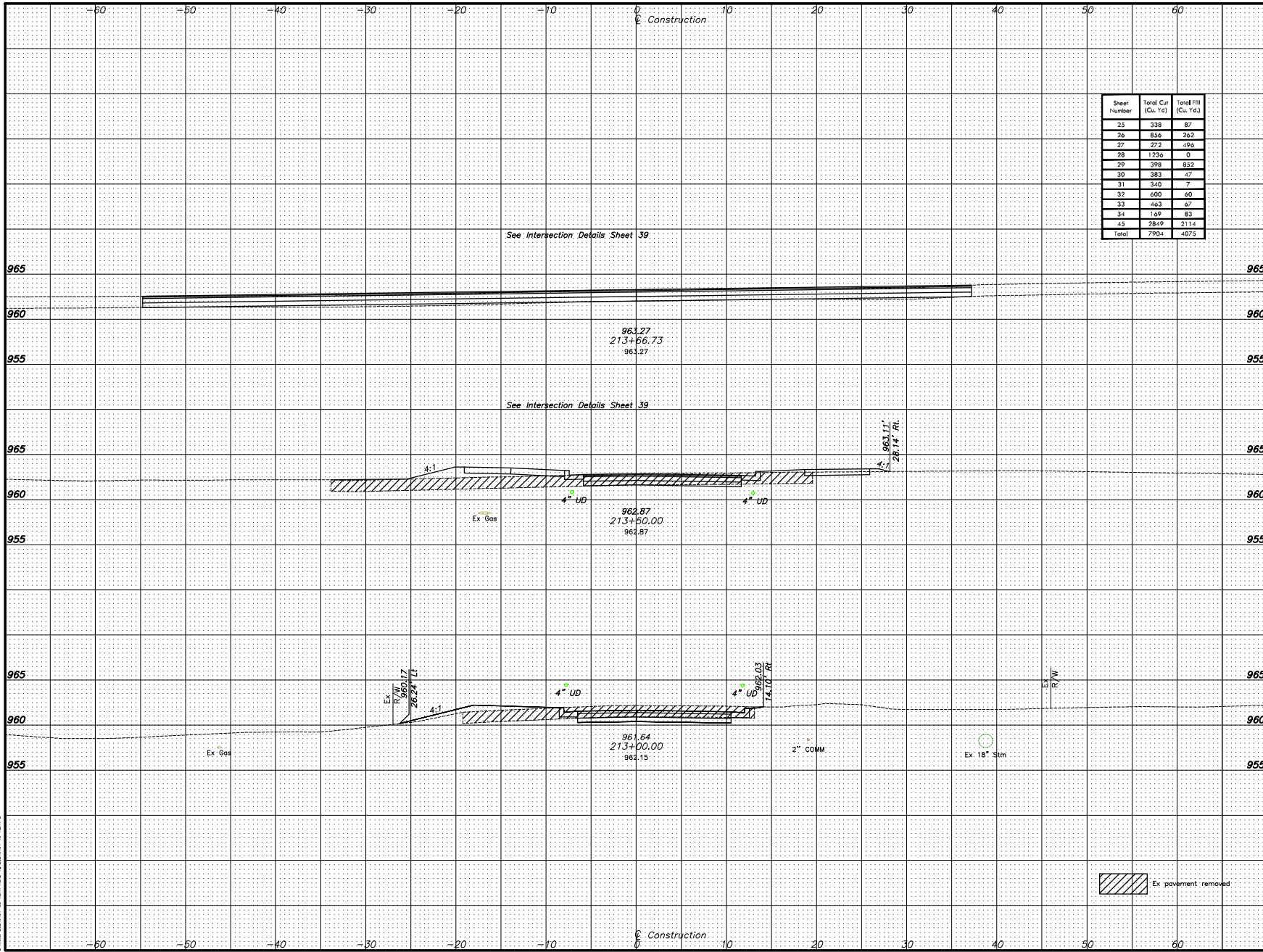
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STA 211+00 TO STA 212+50**

CHESHIRE ROAD RELOCATION

33
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26	856	262
27	272	496
28	1236	0
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31	340	7
32	600	60
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34	169	83
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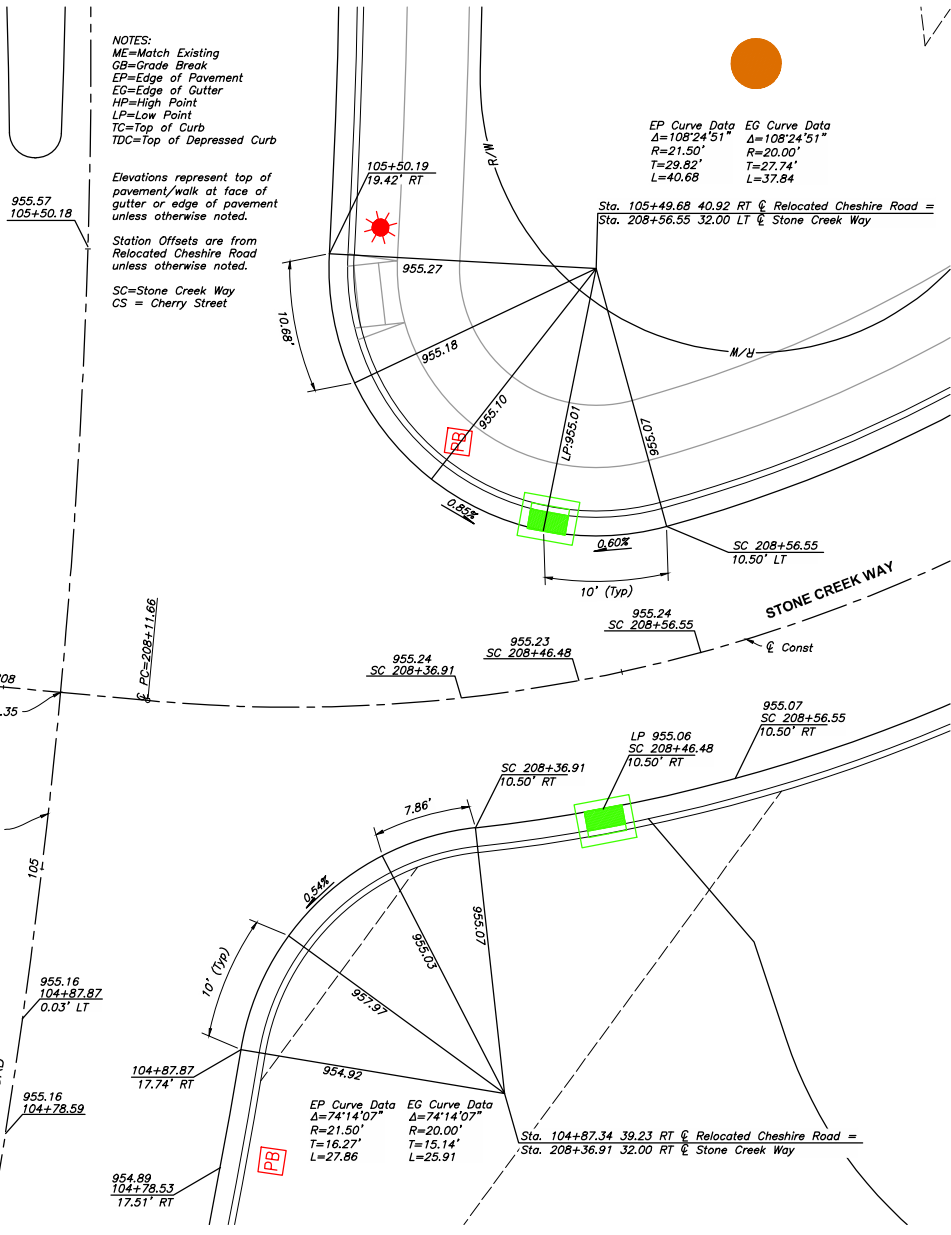
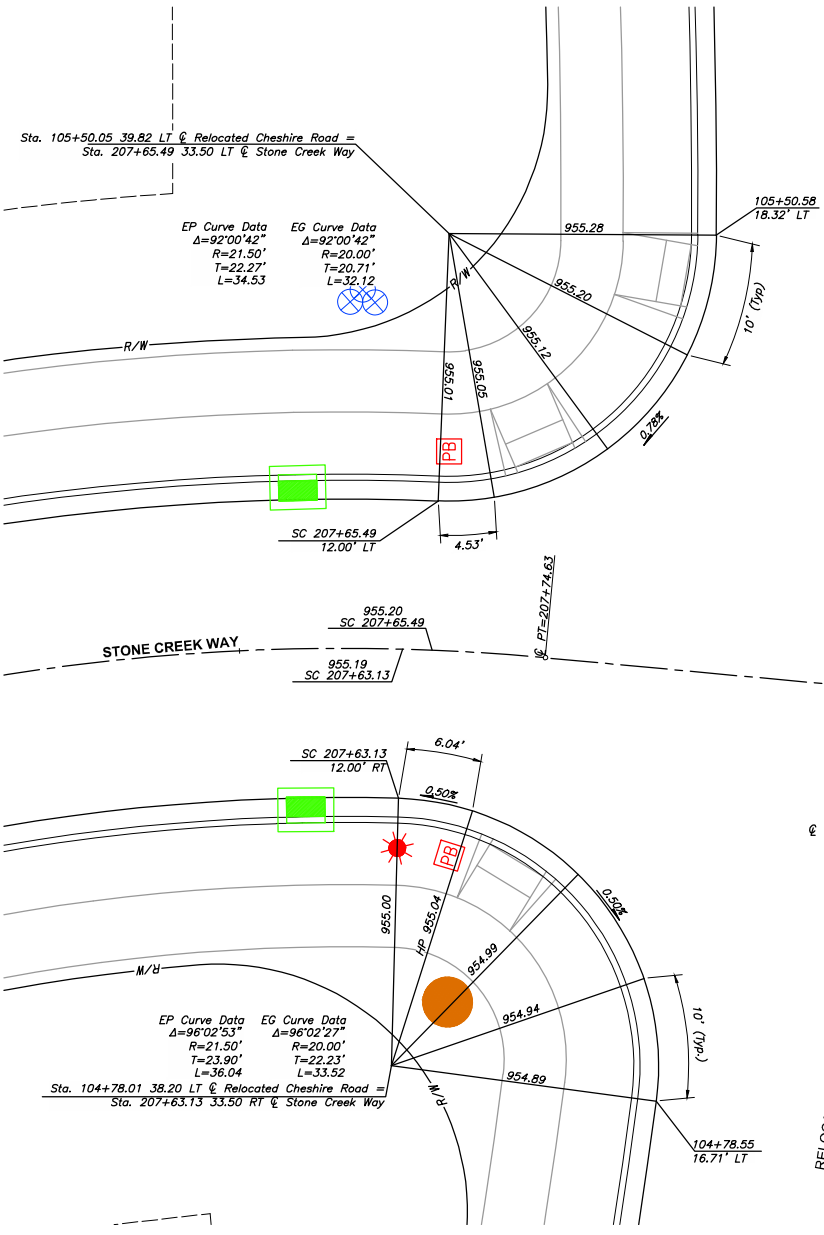
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	Cut	Fill	Cut	Fill
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960				
955	115	0		
965			58	16
960				
955	69	51		
965			111	67
960				
955	50	21		
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CROSS SECTIONS
 STA 213+00 TO STA 213+85.18

CHESHIRE ROAD RELOCATION

34
 65

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NOTES:
 ME=Match Existing
 GB=Grade Break
 EP=Edge of Pavement
 EG=Edge of Gutter
 HP=High Point
 LP=Low Point
 TC=Top of Curb
 TDC=Top of Depressed Curb

Elevations represent top of pavement/walk at face of gutter or edge of pavement unless otherwise noted.

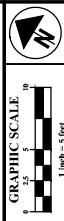
Station Offsets are from Relocated Cheshire Road unless otherwise noted.

SC=Stone Creek Way
 CS = Cherry Street

EP Curve Data EG Curve Data
 $\Delta=108^{\circ}24'51''$ $\Delta=108^{\circ}24'51''$
 $R=21.50'$ $R=20.00'$
 $T=29.82'$ $T=27.74'$
 $L=40.68$ $L=37.84$

EP Curve Data EG Curve Data
 $\Delta=96^{\circ}02'53''$ $\Delta=96^{\circ}02'27''$
 $R=21.50'$ $R=20.00'$
 $T=23.90'$ $T=22.23'$
 $L=36.04$ $L=33.52$

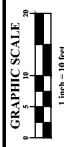
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 $\Delta=74^{\circ}14'07''$ $\Delta=74^{\circ}14'07''$
 $R=21.50'$ $R=20.00'$
 $T=16.27'$ $T=15.14'$
 $L=27.86$ $L=25.91$



CALCULATED	JUB	CHECKED	MUS
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INTERSECTION DETAILS

CHESHIRE ROAD RELOCATION



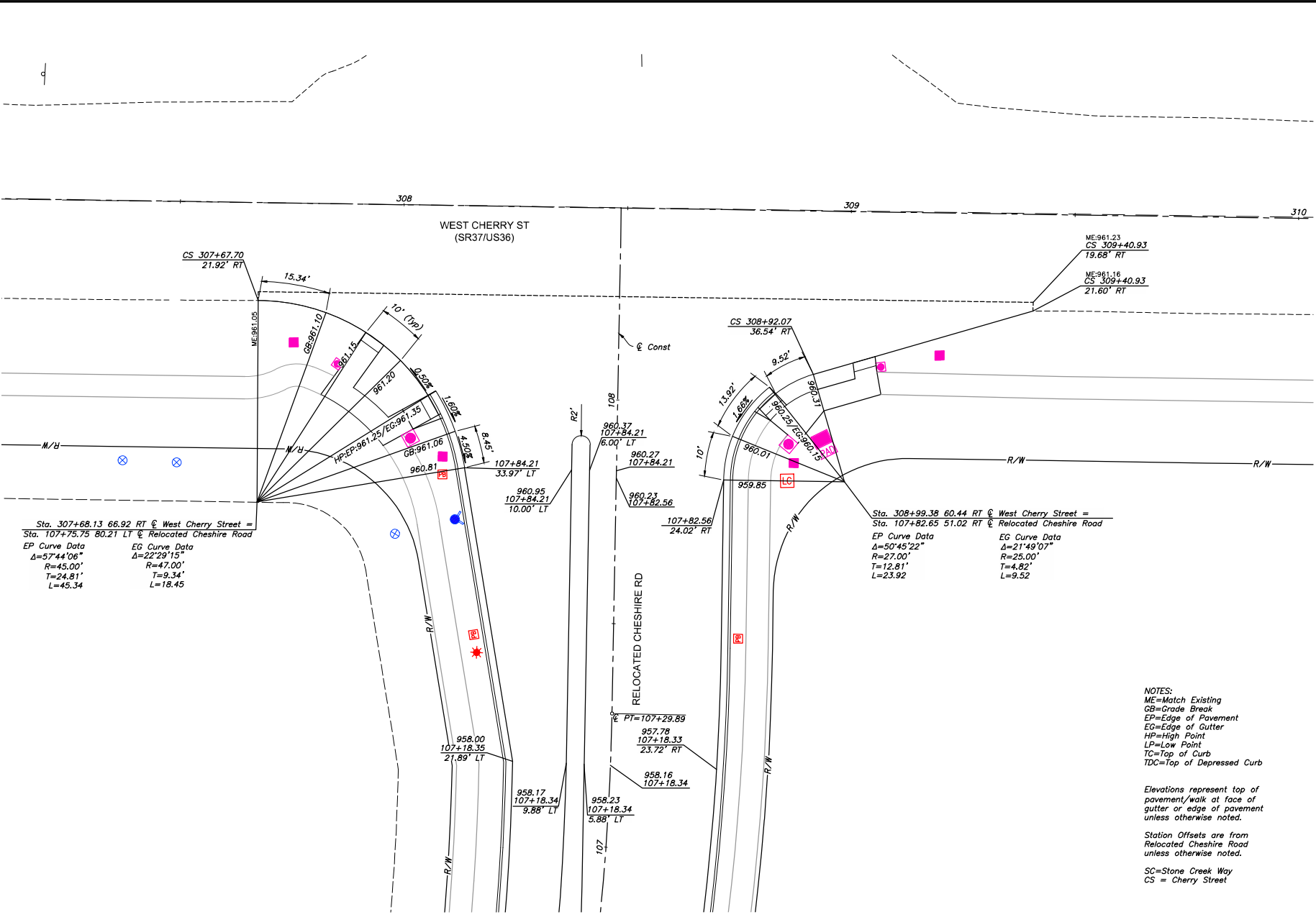
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JUB
CHECKED
MUS

INTERSECTION DETAILS

CHESHIRE ROAD RELOCATION

36
65

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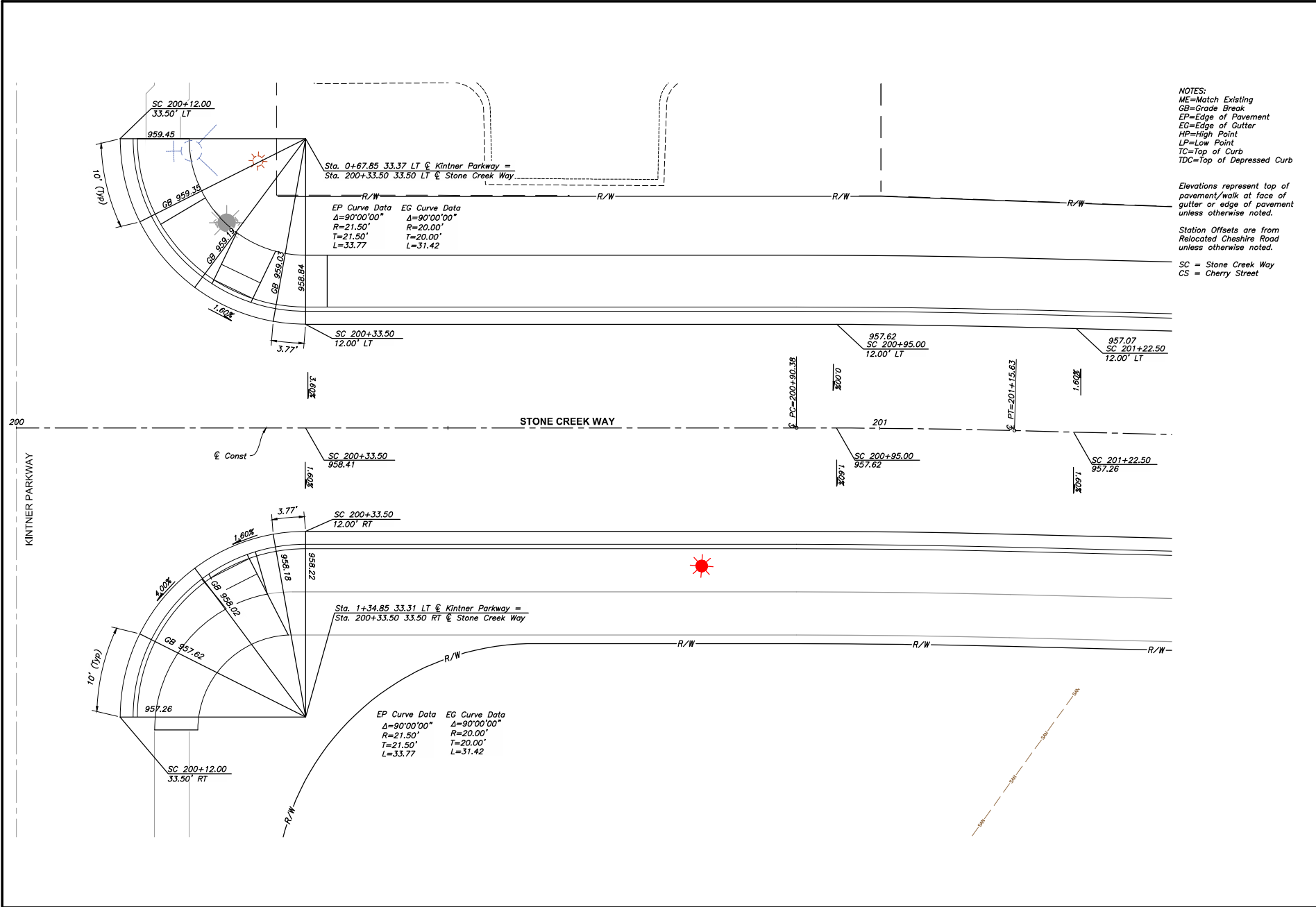
NOTES:
 ME=Match Existing
 GB=Grade Break
 EP=Edge of Pavement
 EG=Edge of Gutter
 HP=High Point
 LP=Low Point
 TC=Top of Curb
 TDC=Top of Depressed Curb

Elevations represent top of pavement/walk at face of gutter or edge of pavement unless otherwise noted.

Station Offsets are from Relocated Cheshire Road unless otherwise noted.

SC=Stone Creek Way
 CS = Cherry Street

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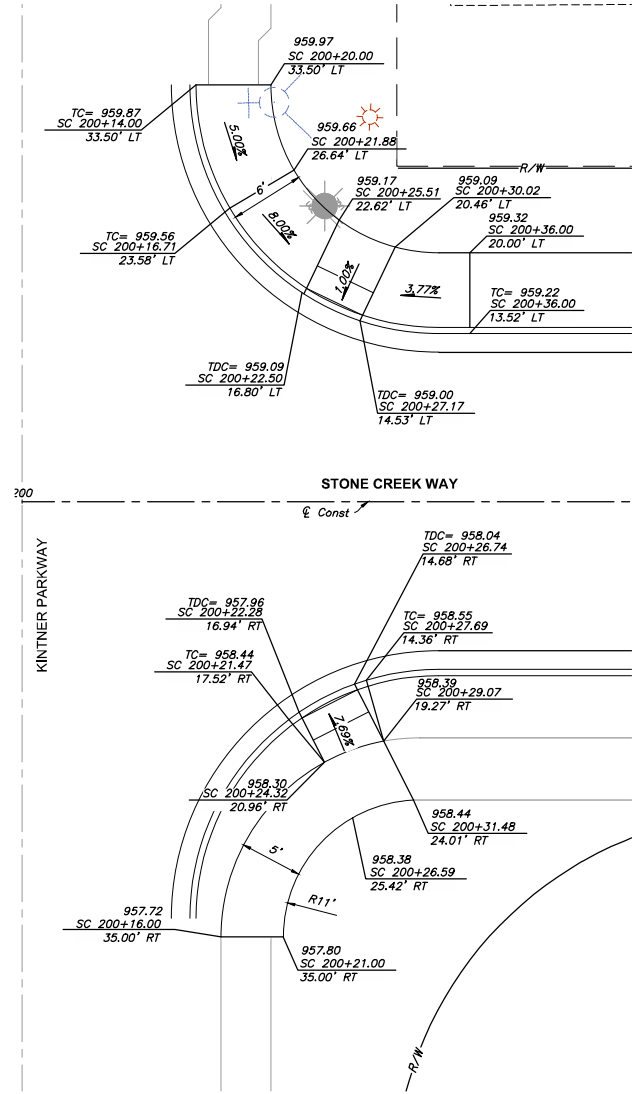


NOTES:
 ME=Match Existing
 GB=Grade Break
 EP=Edge of Pavement
 EG=Edge of Gutter
 HP=High Point
 LP=Low Point
 TC=Top of Curb
 TDC=Top of Depressed Curb

Elevations represent top of pavement/walk at face of gutter or edge of pavement unless otherwise noted.
 Station Offsets are from Relocated Cheshire Road unless otherwise noted.
 SC = Stone Creek Way
 CS = Cherry Street

CHESHIRE ROAD RELOCATION
INTERSECTION DETAILS
 GRAPHIC SCALE: 1" = 50'
 CALCULATED: JUB
 CHECKED: MUS

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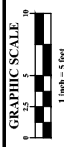


NOTES:
 ME=Match Existing
 GB=Grade Break
 EP=Edge of Pavement
 EG=Edge of Gutter
 HP=High Point
 LP=Low Point
 TC=Top of Curb
 TDC=Top of Depressed Curb

Elevations represent top of pavement/walk at face of gutter or edge of pavement unless otherwise noted.

Station Offsets are from Relocated Cheshire Road unless otherwise noted.

SC= Stone Creek Way
 CS = Cherry Street

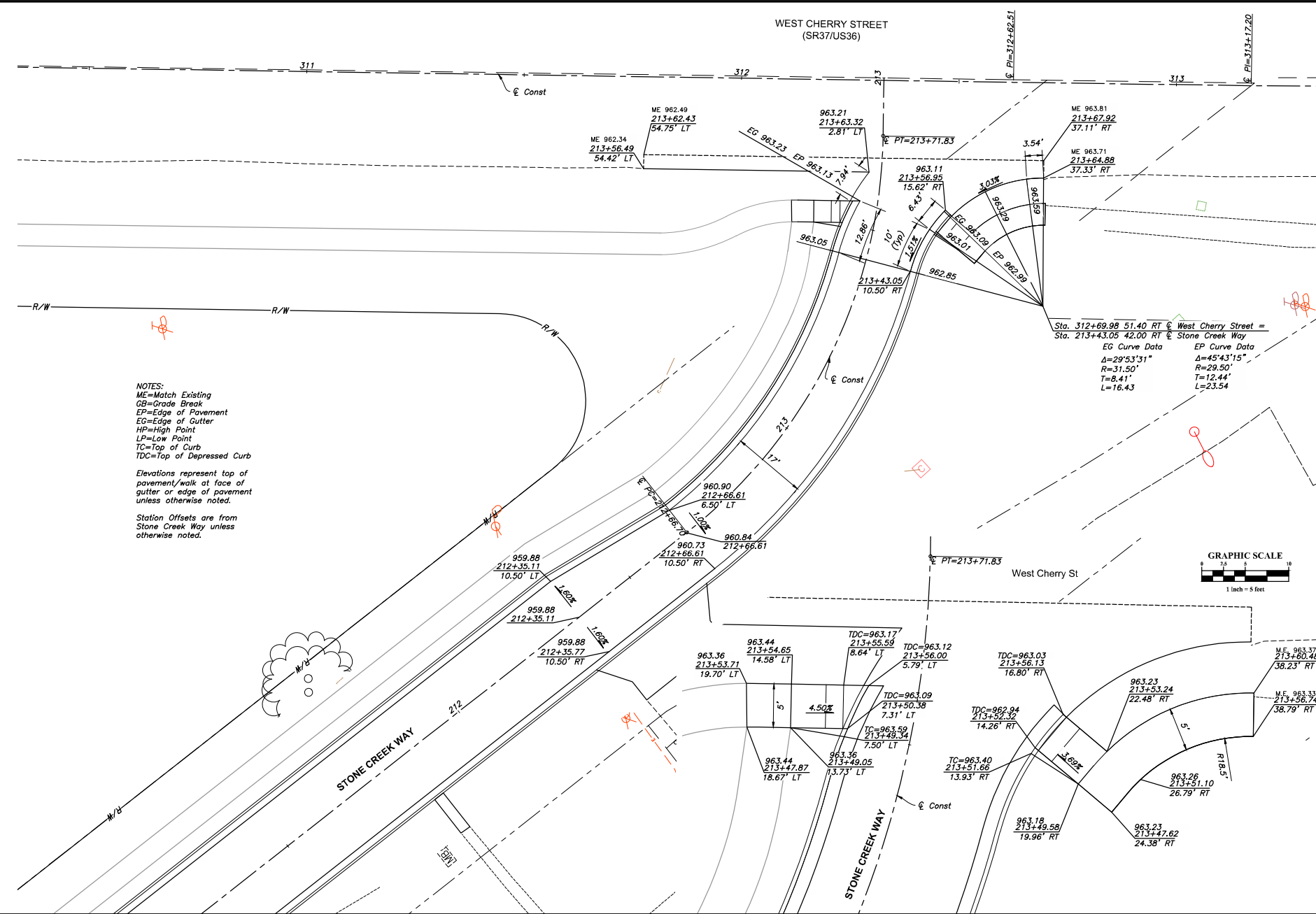


CALCULATED	JUB	CHECKED	MUS
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CURB RAMP DETAILS

CHESHIRE ROAD RELOCATION

WEST CHERRY STREET
(SR37/US36)



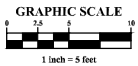
NOTES:
 ME=Match Existing
 OB=Grade Break
 EP=Edge of Pavement
 EG=Edge of Gutter
 HP=High Point
 LP=Low Point
 TC=Top of Curb
 TDC=Top of Depressed Curb

Elevations represent top of pavement/walk at face of gutter or edge of pavement unless otherwise noted.

Station Offsets are from Stone Creek Way unless otherwise noted.

Sta. 312+69.98 51.40 RT @ West Cherry Street =
 Sta. 213+43.05 42.00 RT @ Stone Creek Way

EG Curve Data	EP Curve Data
A=29°53'31"	A=45°43'15"
R=31.50'	R=29.50'
T=8.41'	T=12.44'
L=16.43	L=23.54

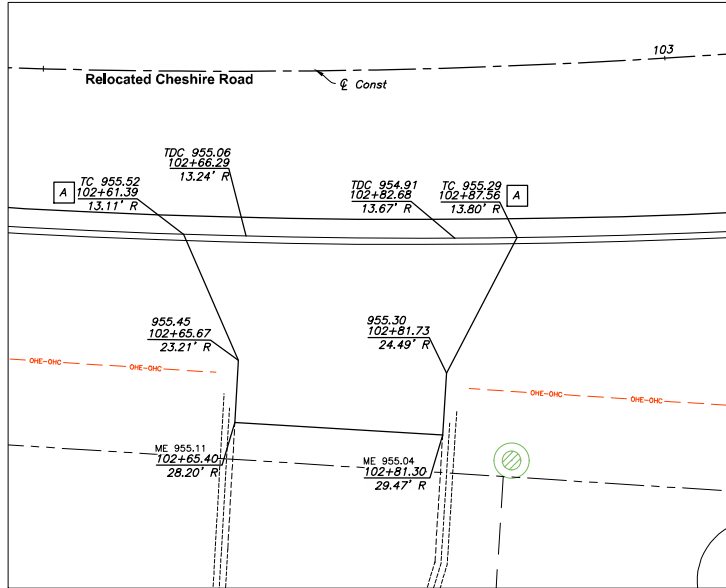


INTERSECTION DETAILS

CHESHIRE ROAD RELOCATION

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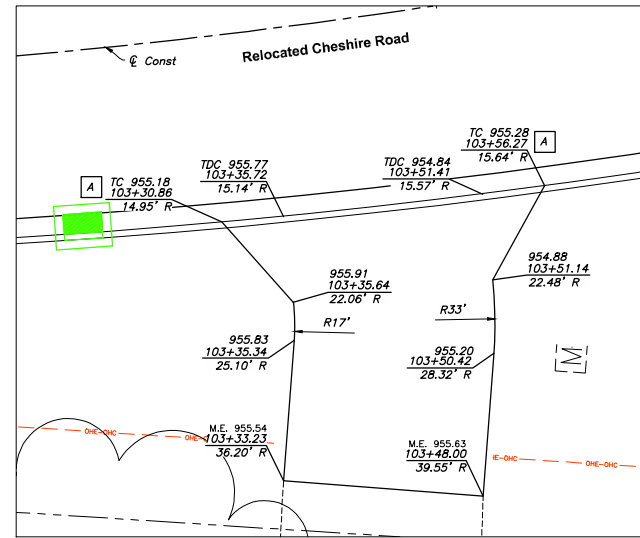
575 Cheshire Road, Sunbury, Ohio 43074

NOTES:

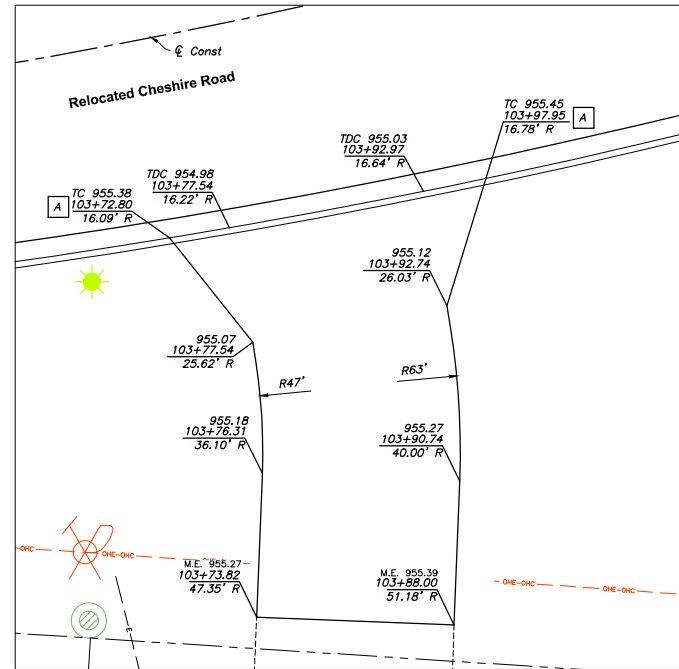
TDC = Top of Depressed Curb
 ME = Match Existing
 TC = Top of Casting

Elevations represent top of pavement unless otherwise noted.

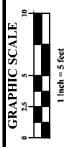
A Curb shall transition from 6" to 1" over 6".



563 Cheshire Road, Sunbury, Ohio 43074



551 Cheshire Road, Sunbury, Ohio 43074



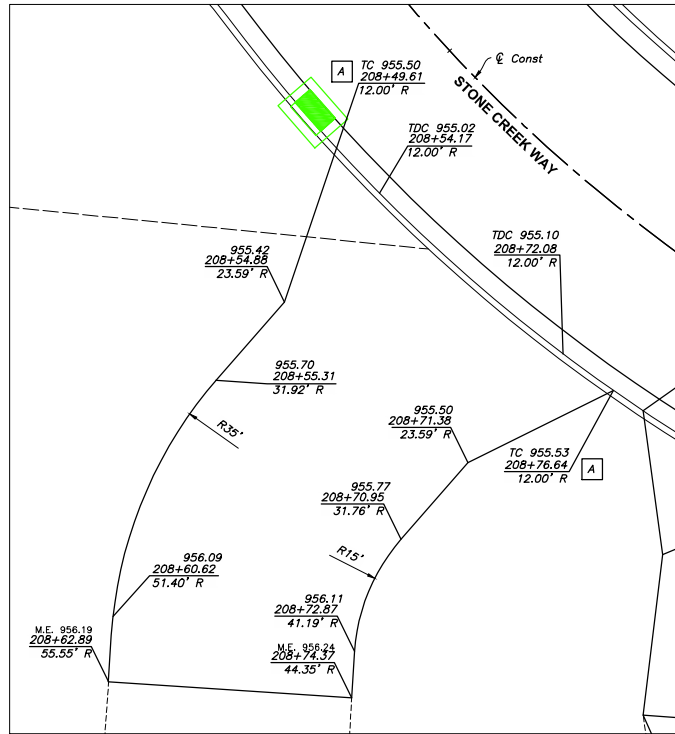
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DRIVE DETAILS

CHESHIRE ROAD RELOCATION

41
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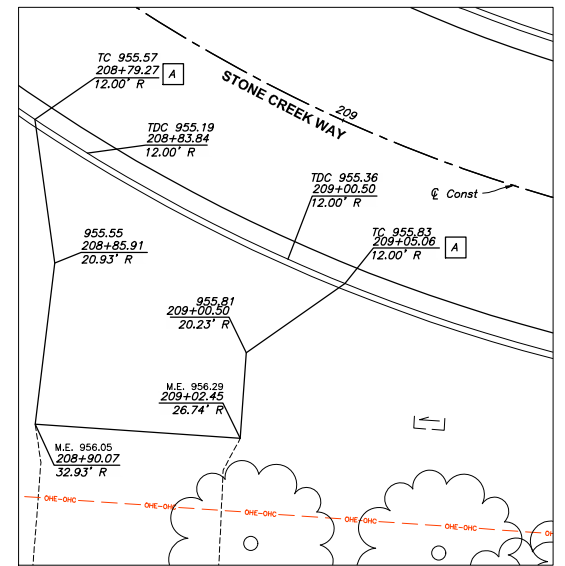
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 P:\2024\44\Cheshire\20240920\20240920.dwg, User: JAF, Date: 9/20/2025, 12:57 PM, Unit: Feet, Plot: 10/17/2025 3:08 PM



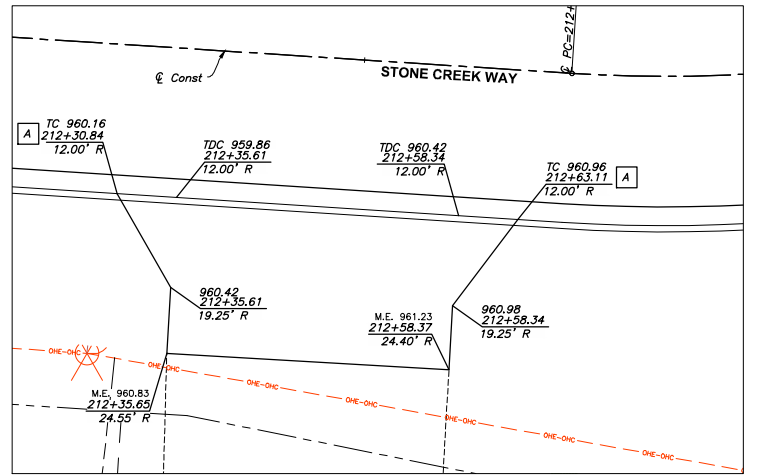
537 Cheshire Road, Sunbury, Ohio 43074

NOTES:
 TDC = Top of Depressed Curb
 ME = Match Existing
 TC = Top of Casting
 Elevations represent top of pavement unless otherwise noted.

A Curb shall transition from 6" to 1" over 6".



523 Cheshire Road, Sunbury, Ohio 43074



579 West Cherry Street, Sunbury, Ohio 43074

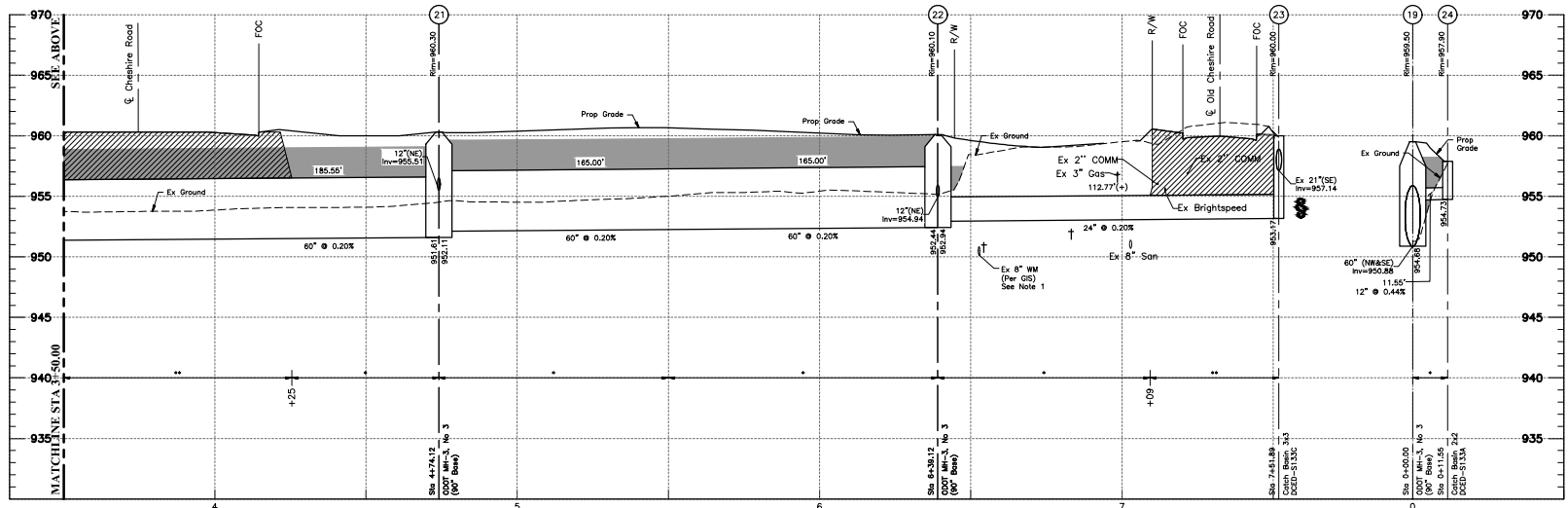
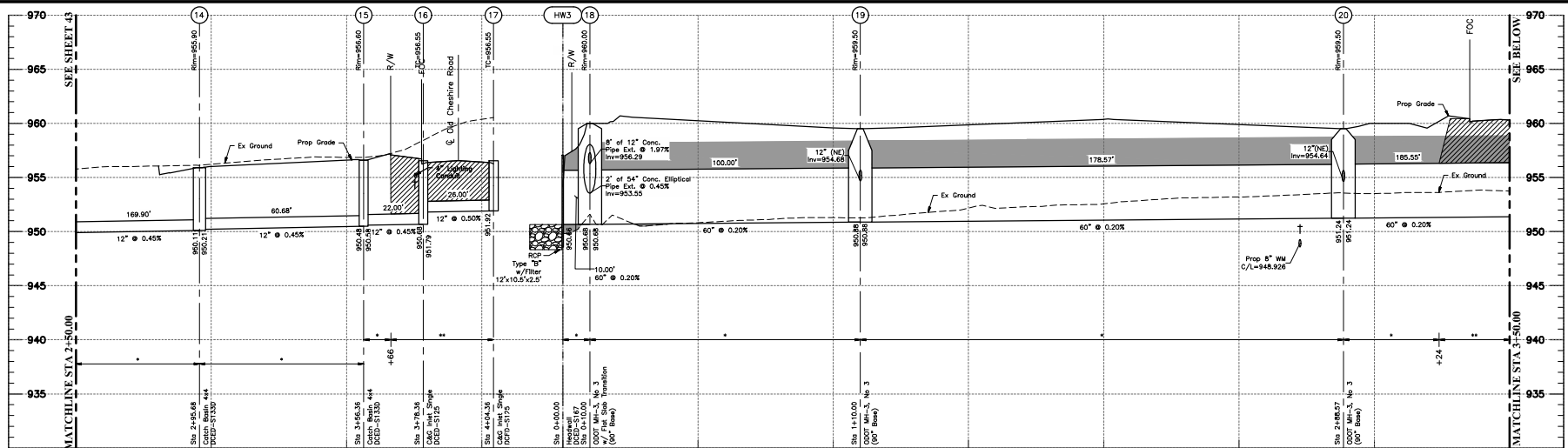
DRIVE DETAILS

CHESHIRE ROAD RELOCATION

42
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SEPTMBER, 2025

C:\Users\jcm\OneDrive\Documents\2025\09\2025_09_24\0925_1927_Plan.dwg
 User: jcm
 Date: 9/24/2025 10:00:00 AM
 Plot Date: 9/24/2025 10:00:00 AM
 Plot Scale: 1" = 50.00'




NOTES:


All elevations on this plan are based on NAVD 1988 datum, unless otherwise noted.

All street crossings shown in profiles are public streets.

All fills are to be placed a minimum of 2.5' above the proposed storm sewer prior to the start of sewer construction per Item 203 of CMS.

Denoted thus: 


† Contractor shall maintain a 1.5' minimum clearance from outside of pipe to outside of pipe for all utility crossings unless otherwise noted.

 Where plans provide for a proposed conduit to be connected to or cross over or under an existing sewer or underground utility, the contractor shall locate the existing pipes or utilities both as to line and grade before starting to lay the proposed conduit.

Contractor to verify invert/top of pipe and location of existing utility before construction. If there is a discrepancy between the contractor's information and the plan information shown, the contractor is to contact the engineer of record prior to the start of construction.

* Compacted Backfill, Per CMS Item 911

** Compacted Granular Backfill, Per CMS Item 912

Denoted thus: 

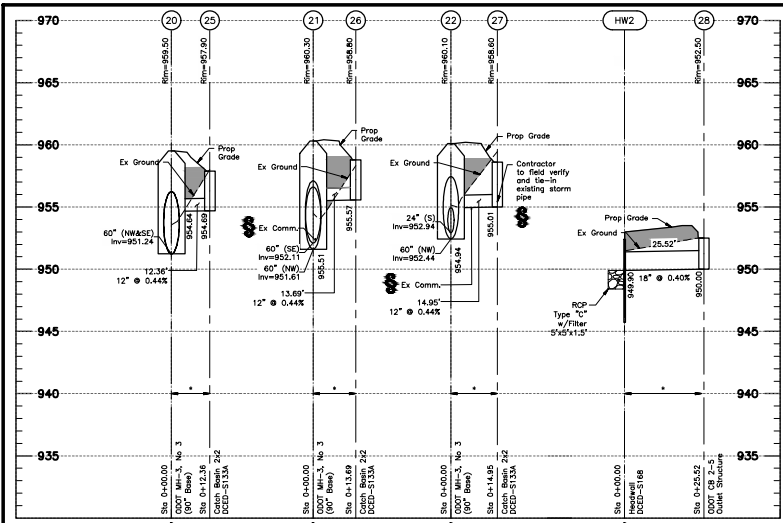
(+) Pipe runs are to be sanitary grade (watertight) and have watertight joints per CMS 901.15, and Trench Dams per CMS 901.11. This applies to full pipe length, from structure to structure.

Note 1 Existing Water Main shown per provided GIS linework. Contractor to coordinate with Del-Co on locating and lowering Water Main.

CALCULATED	HORIZ.	1/8" = 10'
✓	VERT.	1/8" = 5'
CHECKED	M/S	

STORM PROFILES

CHESHIRE ROAD RELOCATION



STRUCTURE COORDINATES				
STRUCTURE #	NORTHING - PLAN	EASTING - PLAN	NORTHING - AS BUILT	EASTING - AS BUILT
1	212949.1788	1863977.9978		
2	212986.1033	1864106.1941		
3	212984.9190	1864112.7975		
4	212931.6831	1864124.7298		
5	212928.0508	1864181.1973		
6	212942.3244	1864199.7816		
7	212931.8034	1863980.0824		
8	212903.6390	1863982.7955		
9	213006.0416	1864134.6649		
10	213020.2954	1864156.7285		
11	213040.1938	1864190.2713		
12	212953.3008	1864254.9466		
13A	213349.6039	1863526.6945		
13B	213283.9490	1863597.9322		
14	213332.9324	1863760.6131		
15	213374.2971	1863716.2190		
16	213390.3929	1863731.2165		
17	213409.4152	1863748.9408		
18	213391.8769	1864122.1445		
19	213323.9684	1864195.5412		
20	213202.6766	1864326.6052		
21	213078.3383	1864462.4993		
22	212963.5971	1864582.9751		
23	212883.7857	1864567.3077		
24	213332.5139	1864203.3063		
25	213211.8267	1864334.9100		
26	213066.3851	1864471.8025		
27	212974.5671	1864593.1332		
28	213212.1707	1863698.9748		
29	212861.7798	1864391.4701		
30	212860.1793	1864422.7721		
C1	213282.4670	1863805.7119		
C2	213303.5331	1863899.3720		
Ex 26		213231.0708	1863501.1849	
Ex 27		213285.8446	1863401.3186	
Ex 32		213327.7340	1863507.7890	
Ex C1		213282.4670	1863805.7119	
Ex C2		213303.5331	1863899.3720	
Ex HW6		212924.3472	1864159.3945	
Ex HW7		212921.6311	1864201.1278	
HW1	212979.6812	1863945.2615		
HW2	213236.2799	1863690.6013		
HW3	213383.0268	1864117.4889		
HW6	212924.3472	1864159.3945		
HW7	212921.6311	1864201.1278		

STORM SEWER TABLE			
LINE	BEARING	LENGTH	SIZE
HW1-1	S47°01'23"E	44.74'	24"
1-2	N73°48'44"E	132.45'	24"
2-3	S19°44'36"E	22.51'	15"
3-4	S19°44'36"E	36.31'	15"
4-5	S84°18'18"E	56.75'	12"
5-6	N48°44'20"E	24.72'	12"
1-7	S06°02'28"E	17.50'	12"
7-8	S05°00'00"E	28.29'	12"
2-9	N05°15'30"E	36.86'	15"
9-10	N08°03'48"E	26.00'	15"
10-11	N09°19'19"E	39.00'	12"
11-12	S36°39'38"E	108.32'	12"
Ex 32-13A	N47°00'31"E	28.91'	12"
13A-13B	S47°20'07"E	96.88'	12"
13B-14	N73°14'34"E	169.80'	12"
14-15	N47°01'23"W	60.68'	12"
15-16	N42°58'17"E	22.00'	12"
16-17	N42°58'17"E	26.00'	12"
HW3-18	N27°44'48"E	10.00'	60"
18-19	S47°13'12"E	100.00'	60"
19-20	S47°13'12"E	178.50'	60"
20-21	S47°05'13"E	185.55'	60"
21-22	S46°03'58"E	165.00'	60"
22-23	S13°09'22"W	112.77'	24"
19-24	N42°13'38"E	11.55'	12"
20-25	N42°13'38"E	12.36'	12"
21-26	N42°47'58"E	13.69'	12"
22-27	N42°47'58"E	14.95'	12"
Ex 8 - 29	S19°09'10"E	25.52'	18"
-29	S68°39'17"E	9.22'	15"
29-30	S67°04'23"E	31.34'	15"

NOTES:

All elevations on this plan are based on NAVD 1988 datum, unless otherwise noted.

All street crossings shown in profiles are public streets.

All fills are to be placed a minimum of 2.5' above the proposed storm sewer prior to the start of sewer construction per Item 203 of CMS.

Denoted thus:

† Contractor shall maintain a 1.5' minimum clearance from outside of pipe to outside of pipe for all utility crossings unless otherwise noted.

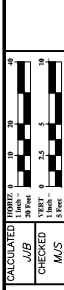
Where plans provide for a proposed conduit to be connected to or cross over or under an existing sewer or underground utility, the contractor shall locate the existing pipes or utilities both as to line and grade before starting to lay the proposed conduit. Contractor to verify invert/top of pipe and location of existing utility before construction. If there is a discrepancy between the contractor's information and the plan information shown, the contractor is to contact the engineer of record prior to the start of construction.

* Compacted Backfill, Per CMS Item 911

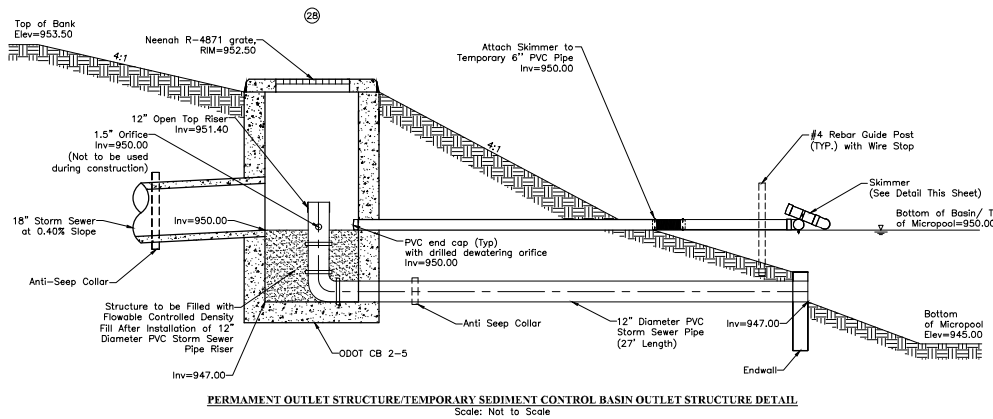
** Compacted Granular Backfill, Per CMS Item 912

Denoted thus:

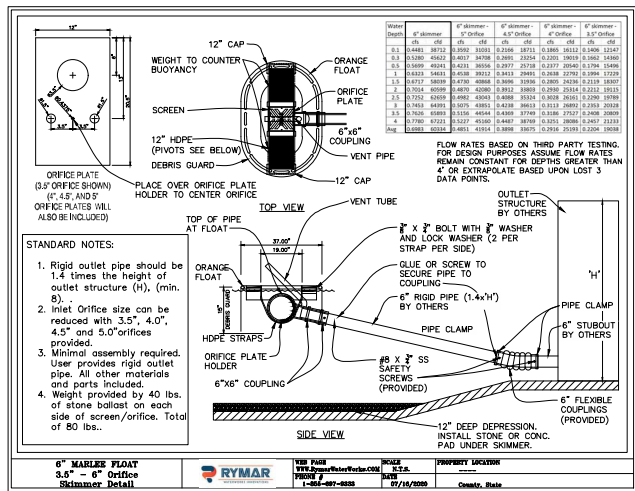
(+) Pipe runs are to be sanitary grade (watertight) and have watertight joints per CMS 901.15, and Trench Dams per CMS 901.11. This applies to full pipe length, from structure to structure.



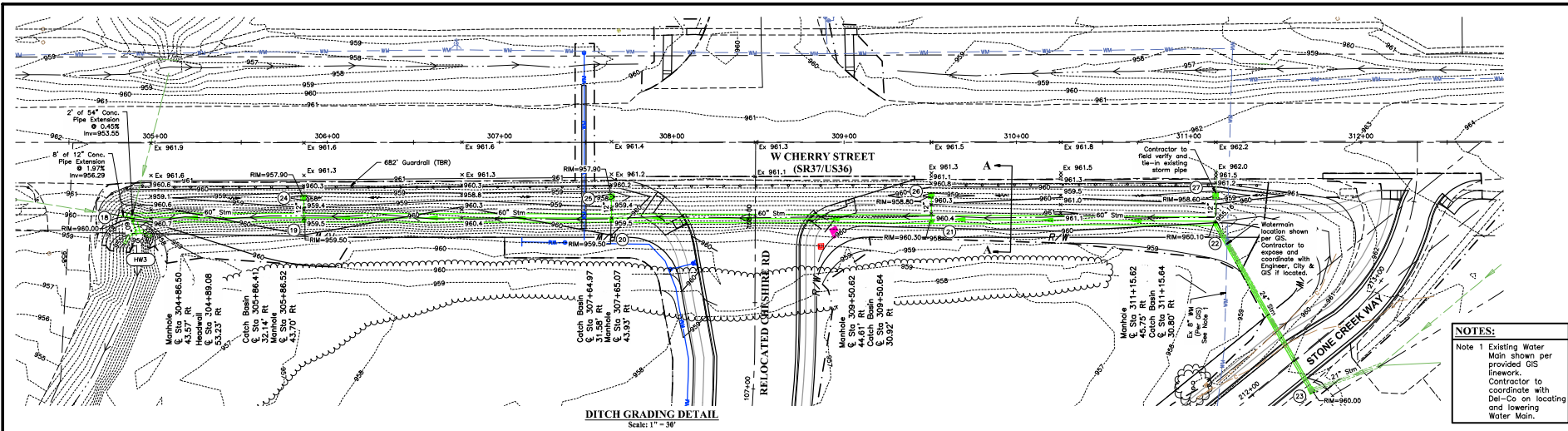
STORM PROFILES



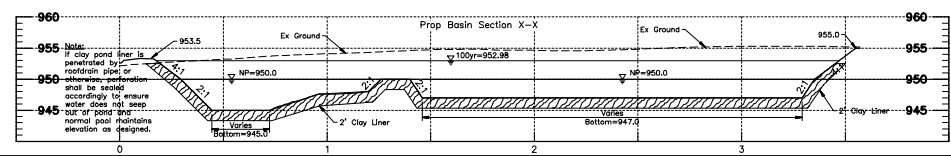
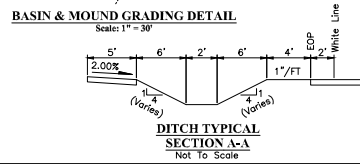
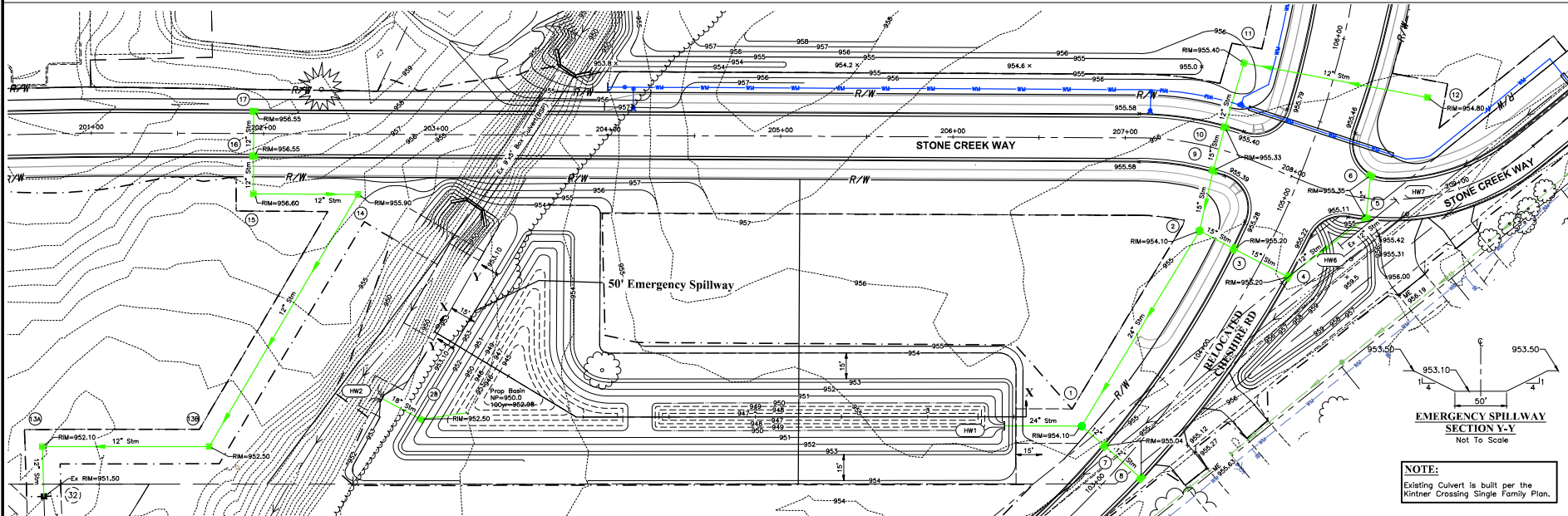
PERMANENT OUTLET STRUCTURE/TEMPORARY SEDIMENT CONTROL BASIN OUTLET STRUCTURE DETAIL
Scale: Not to Scale



CHESHIRE ROAD RELOCATION



NOTES:
Note 1 Existing Water Main shown per provided GIS line work. Contractor to coordinate with Del-Co on locating and lowering Water Main.



CALCULATED	DATE	BY	CHECKED	DATE

GRADING DETAILS

CHESHIRE ROAD RELOCATION

46
65

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 3 Views: 202404 - CS - RFR - 202404 - CS - RFR - 202404 - CS - RFR

GENERAL
 These specifications, together with the accompanying plans, are intended to describe the type, size, and location of the products and materials to be provided and installed under various bid items related to the traffic control. The Contractor shall furnish and install traffic control devices and related materials in the compliance with these plans and specifications, as well as the 2023 Ohio Department of Transportation Construction and Material Specifications. The Ohio Manual of Uniform Traffic Control Devices for Streets and Highway, and The Standard Construction Drawings issued by the Ohio Department of Transportation. These specifications set forth the minimum performance and operating requirements of the traffic control items referred to herein.

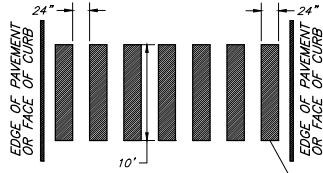
ITEM 621 RPM
ITEM 621 RAISED PAVEMENT MARKER REMOVED
ITEM 621 RPM, MISC.: RPM REFLECTOR REMOVED AND REPLACED
 Under the items of work listed, Raised Pavement Markers (RPMs) within the Cherry Street Corridor between Sta. 306+08 and 315+69 shall be modified to conform with the spacing and color requirements specified in ODOT Standard Construction Drawing TC-65.10 and TC-65.11, as much as practical while minimizing removal of existing RPMs.

Existing RPMs within the Cherry Street corridor shall be modified as outlined below:
 -Only existing RPMs located in the center of the signalized intersection, between the eastbound and westbound stop bars (Sta. 307+83 to 309+14), shall be removed. Voids resulting from the removal shall be filled with epoxy.
 -Reflectors shall be changed for RPMs located in locations where pavement markings are changed (For example, changing from DY to CH).

New RPMs shall be installed to supplement the existing RPMs on the eastbound and westbound approaches to the new signalized intersection to conform with the spacing requirements outlined in ODOT standard construction drawings.

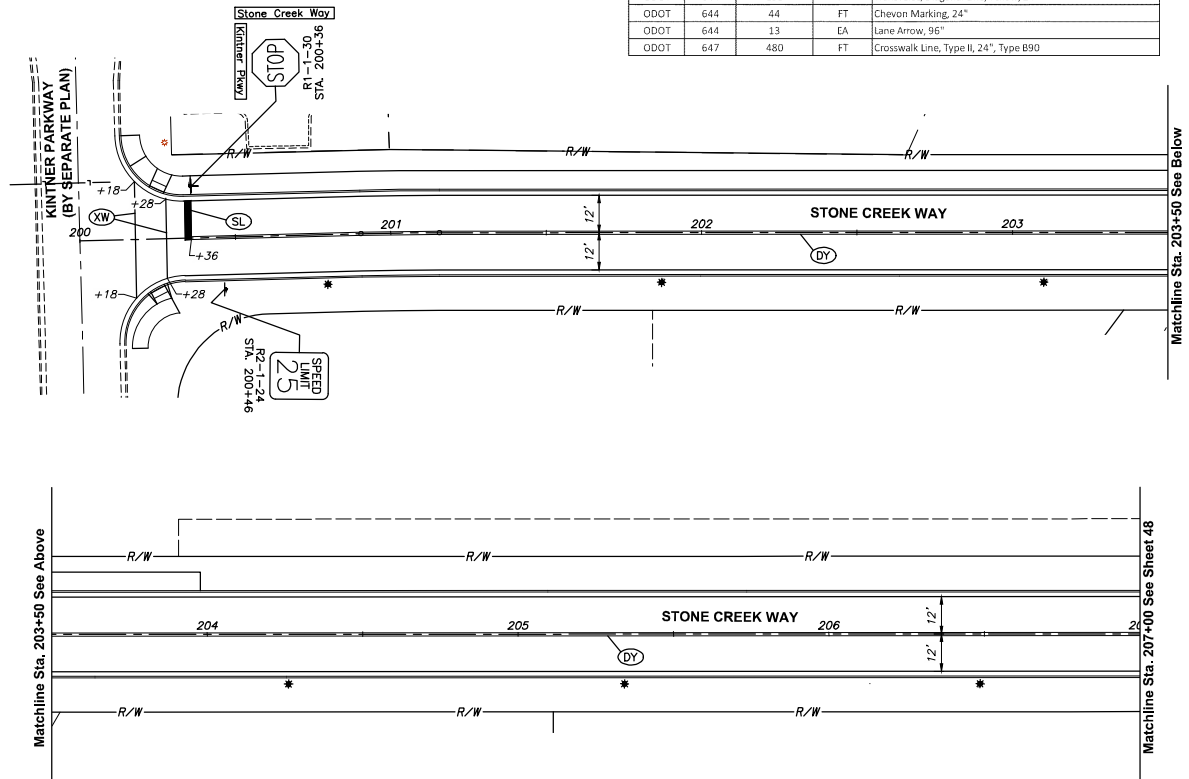
RPMs are only required on Cherry Street. RPMs shall not be installed on any other roadway.

TRAFFIC CONTROL ESTIMATE OF QUANTITIES				
Spec	Item No.	Estimated Quantity	Unit	Item Description
ODOT	621	16	EA	RPM
ODOT	621	4	EA	Raised Pavement Marker Removed
ODOT	621	8	EA	RPM, Misc.: RPM Reflector Removed and Replaced
COC	627	25	EA	Reboundable Traffic Post - Yellow Installed
ODOT	630	378	FT	Ground Mounted Support, No. 3 Post
ODOT	630	184	SF	Sign, Flat Sheet
ODOT	630	1	EA	Removal of Ground Mounted Sign and Reerection
ODOT	630	9	EA	Removal of Ground Mounted Sign and Storage
ODOT	630	7	EA	Removal of Ground Mounted Post Support and Disposal
ODOT	630	6	EA	Sign, Double-Faced, Street Name
ODOT	644	6	EA	Removal of Pavement Marking
ODOT	644	2300	FT	Removal of Pavement Marking
ODOT	644	0.05	MILE	Center Line, Solid and Dashed, 4"
ODOT	644	0.51	MILE	Center Line, Double Yellow, 4"
ODOT	644	0.10	MILE	Edge Line, Yellow 6"
ODOT	644	851	FT	Channelizing Line, 8"
ODOT	644	102	FT	Crosswalk Line, 12"
ODOT	644	201	FT	Stop Line, 24"
ODOT	644	193	FT	Transverse/Diagonal Line, Yellow, 24"
ODOT	644	44	FT	Chevron Marking, 24"
ODOT	644	13	EA	Lane Arrow, 96"
ODOT	647	480	FT	Crosswalk Line, Type II, 24", Type B90



CROSSWALK, 24" DETAIL

ITEM 647
 HIGH SKID RESISTANT
 PREFORMED PAVEMENT
 MARKING MATERIAL



LINE SPECIFICATIONS	
YE	Edge Line, 4" Yellow
WE	Edge Line, 4" White
CM	Chevron Marking, 24" White
YT	Transverse Line, 24" Yellow (ODOT SCD TC-71.10)
DY	Center Line, 4" Solid Double Yellow
CSD	Center Line, 4" Solid & Dashed Yellow
CH	Channelizing Line, 8" White
SL	Stop Line, 24" White
XW	Crosswalk Line, 12" White
XW2	Crosswalk Line, 24" White, See Detail (ODOT SCD TC-74.10)

PAVEMENT MARKING LEGEND

- (DY) EXISTING PAVEMENT MARKING
- (DY) PROPOSED PAVEMENT MARKING, ITEM 644
- (DY) PROPOSED PAVEMENT MARKING, ITEM 647
- (R) REMOVAL OF PAVEMENT MARKING



CALCULATED
 RLS
 CHECKED
 DFM

TRAFFIC CONTROL PLAN

CHESHIRE ROAD RELOCATION

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GENERAL

These specifications, together with the accompanying plans, are intended to describe the type, size, and location of the products and materials to be provided and installed under various bid items related to traffic control and highway lighting. The Contractor shall furnish and install traffic control and highway lighting devices and related materials in compliance with these plans and specifications, as well as the 2023 Ohio Department of Transportation Construction and Material Specifications, the Ohio Manual of Uniform Traffic Control Devices for Streets and Highways, and the Standard Construction Drawings issued by the Ohio Department of Transportation. These specifications set forth the minimum performance and operating requirements of the traffic control and highway lighting items referred to herein.

PLAN AND SPECIFICATION COMPLIANCE

The Contractor shall furnish and install traffic signal devices in compliance with these plans and specifications, the 2023 ODOT Construction and Material Specifications including all supplemental specifications, the Ohio Manual of Uniform Traffic Control Devices for Streets and Highways, and the "TC" and "HL" standard construction drawings issued by ODOT. These specifications set forth the minimum design and operating requirements for traffic signal and highway lighting equipment.

Traffic signal control and highway lighting equipment shall meet or exceed the standards specified in the following documents:

- a) Specifications listed in this plan.
b) NEMA Standards Publication no. TS1-1989 and/or TS2-2003 (or current NEMA issue)
c) 2023 ODOT Construction and Material Specifications.

In case of a conflicting specification statement, the specification document hierarchy shall be in the order listed from (A), highest, to (C), lowest.

UNDERGROUND UTILITIES

The location of the underground utilities shown on these plans are as obtained from the owners of the utility as required by Ohio Revised Code section 153.64. The Ohio Department of Transportation, City of Sunbury, and the Engineer assume no responsibility for the accuracy of the location or the depths of the underground facilities shown on these plans. Support, protection, and restoration of all existing utilities and appurtenances shall be the Contractor's responsibility. The cost of this work shall be included in the bid price for the various items of work. It shall be the Contractor's responsibility to notify the Ohio Utilities Protection Service (OUPS) so existing facilities can be marked prior to construction.

TRANSITION TO SIGNAL CONTROL

Responsibility for determining whether signal control is appropriate and justified by current conditions, and for setting the date when any newly signalized intersection is placed into cycling operation, rests with the maintaining agency (City of Sunbury), not the Engineer. When the City of Sunbury agency authorizes cycling operation of a traffic signal at an intersection not currently under signal control, new signals shall flash for 7 days prior to the 10 day performance test and before being placed on regular cycling operation. Ohio Department of Transportation Standard Construction Drawing MT-120.00 shall be implemented for transition to signal control.

Additionally, prior to activating the new traffic signal to stop-and-go mode, all items in the proposed signal plan shall be fully completed, (i.e., vehicle detection, pedestrian signal heads, etc.). If there are constructability issues (i.e., roadway widening, etc.) that prevent the signal from being completed prior to activation, it shall be brought to the attention of the Project Engineer. The Engineer will then review, approve or reject proposals to activate the traffic signal prior to completion. The Contractor shall notify the Project Engineer at least 10 working days prior to scheduling the final inspection of the signal installation. Final inspection is not considered complete until the Engineer inspects the traffic signal and issues written approval. If issues are found during the final inspection that effect the safety of the traveling public and/or the efficiency of the intersection, the signal shall not be activated on the proposed date. Any punch list items that are found shall be corrected and re-inspected by the Engineer prior to final acceptance. City forces shall only assume day to day maintenance of the traffic signal after final written acceptance has been issued.

MAINTENANCE OF TRAFFIC SIGNAL INSTALLATIONS

The Contractor shall be responsible for maintaining traffic signal installations within the project under the following conditions:

- 1. The Contractor shall be responsible for existing signal installations which the plan requires the Contractor to adjust, modify, add on to, or remove, or which the Contractor actually adjusts, modifies, or otherwise disturbs, from the time his operations first disturb the installation until the time the installation has subsequently been removed or modified and the work has been accepted.
2. The Contractor shall be responsible for maintenance of new signal installations or devices, installed by the Contractor, from the time of installation until the work is accepted.

The Contractor shall correct as quickly as possible all outages or malfunctions. He shall provide the Engineer such addresses and phone numbers where his maintenance forces can be contacted. The Contractor shall provide one or more persons to receive all calls and dispatch the necessary maintenance forces to correct outages. Such a person or persons may be used to perform other duties as long as prompt attention is given to these calls and a person is readily available continuously 24 hours a day, 7 days a week. All lamp outages, cable outages, electrical failures, equipment malfunctions and misaligned signal heads shall be corrected to the satisfaction of the Engineer with the signal back to service within four hours after the Contractor has been notified of the outage.

In the event new signals are damaged prior to acceptance all damaged equipment except poles and control equipment shall be replaced by the Contractor to the satisfaction of the Engineer with the signal back in service within 8 hours after the Contractor's notification of the outage. If poles and/or control equipment are damaged and must be replaced, the Contractor shall make temporary repairs as necessary to bring the signal back into full operation within the allowed 8-hour period, and shall make permanent repairs or replacement as soon thereafter as possible.

None of the above shall be construed as collective or consecutive outage time periods at any one location. That is, where more than one outage occurs at any one location, then the allotted time limit shall be for the worst single outage.

Where outages are the direct result of a vehicular accident, the response of the Contractor shall be as outlined above. The Contractor shall be responsible for collection of any compensation for this work from those parties responsible for the damage.

Where the Contractor has failed to or cannot respond to an outage or signal equipment malfunction, at these locations within his responsibility, within periods as specified above, the Engineer may invoke the provisions of Section 105.15 and any subsequent costs to the

City for Police Services and Maintenance Services shall be deducted from monies due or to become due the Contractor in accordance with provisions of Section 105.15.

Any signalized intersection, where the signal is out of service due to construction procedures, or due to an outage or malfunction of equipment as described above, shall be placed under officer control.

Any vehicular traffic signal head which will be out of operation shall be covered in the manner described in 632.25.

All costs resulting from the above requirements shall be included in the lump sum price bid for Item 614 Maintaining Traffic, As Per Plan.

PAINT CHIP SUBMITTAL

Prior to any painting, the Contractor shall submit paint samples to both the Engineer and Sunbury officials. Paint samples shall be representative of the color, type and manufacture that will be used for finishing the various items. The Engineer and Sunbury officials shall review the paint samples prior to the commencement of the finishing process.

Paint samples shall be submitted for all proposed traffic signal items called for in this plan set, including, signal supports, signal heads, pedestrian heads, pushbuttons, signs, bracket arms, luminaires and controller cabinets. Any cost associated with providing paint samples shall be incidental to the individual items to be painted.

GROUNDING AND BONDING

The requirements of the Construction and Material Specifications (C&MS) and the TC series of Standard Construction Drawings are modified as follows:

- 1. All metallic parts containing electrical conductors shall be permanently joined to form an Effective Ground Fault Current Path back to the grounded conductor in the power service disconnect switch.
a) Provide an equipment grounding conductor in metallic conduits (725.04) in addition to the conductors specified and bond the conduit to this grounding conductor.
b) When an equipment grounding conductor is required in plastic conduit (725.05), the installation shall include a separate equipment grounding conductor in addition to the conductors specified.
c) Metallic conduit carrying the loop wires from in the pavement to the pull box splice location will only be bonded at the pull box end, and will not contain an equipment grounding conductor.
d) If multiple conduit runs begin and end at the same points, only one equipment grounding conductor is required.
e) If an equipment grounding conductor is needed in conduit between signalized intersections for underground interconnect cable, the grounding system for each signalized intersection will be separated about midway between the intersections.
f) The messenger wire at signalized intersections will be used as the conductive path from corner to corner if conduit is not provided under the roadway. When conduit connects the corners of an intersection, an equipment grounding conductor shall be used in the conduit.
g) Metal pull box lids shall be bonded by attachment of the equipment grounding conductor to the frame diagonal as provided on HL-30.11.
2. Conduits.
The 725.04 conduit shall have grounding bushings installed at all termination points. The bushing material shall be compatible with galvanized steel conduit and the grounding lug material shall be compatible for use with copper wire. Threaded or compression type bushings may be used.
The 725.05 conduit shall have the inside and outside diameters of the conduit deburred at all termination points.
Both ends of metallic conduit shall be bonded to the equipment grounding conductor. Metallic conduit may be bonded to metallic boxes through the use of conduit fittings UL approved for this type of connection, with the box bonded to the equipment grounding conductor.
3. Wire for grounding and bonding.
a) Use insulated, copper wire for the equipment grounding conductor. Bonding jumpers in boxes and enclosures may be bare or insulated copper wire. Wire size shall be as follows:
i. Use 4 AWG between the power service and supports, poles, pedestals, controller or flasher cabinets.
ii. Use a minimum 8 AWG between loop detector pull boxes and the first conduit that requires a larger size as specified in 3.a.i above.
iii. The insulation shall be green or green with yellow stripe(s). For 4 AWG or larger, insulation may also be black with green tape/labels installed at all access points.
b) In a highway lighting system, the equipment grounding conductor shall be the same wire size as the duct cable or distribution cable circuit conductors, with the minimum conductor 4 AWG.
4. Ground rod.
a) A 3/4 inch Schedule 40 PVC conduit will be used in foundations and concrete walls for the grounding conductor (ground wire) raceway to the ground rod. Should metallic conduit be used, both ends of the conduit shall be bonded to the grounding conductor.
b) The typical grounding conductor (ground wire) shall be 4 AWG insulated, copper.
5. The green conductor in signal cables (conductor #4) shall not be used to supply power to a signal; it shall be an exothermic weld butt splice. All equipment ground in equipment ground aluminum heads and it will be unused in plastic heads. Unused conductors shall be grounded in the cabinet. Typical use of conductors is as follows:

Table with 5 columns: COND., NO., COLOR, VEHICLE SIGNAL, PEDESTRIAN SIGNAL. Rows include Black, White, Red, Green, Orange, Blue, White/black stripe and their corresponding signals like #1 Walk, AC neutral, red ball, yellow ball, green arrow, yellow arrow.

6. Power Service and Disconnect Switch.

- a) At the power service location, the grounding conductor (ground wire) from the disconnect switch neutral (AC-) bar to the ground rod shall be a continuous, unspliced conductor. If spliced, it shall be an exothermic weld butt splice.
b) The service neutral (AC-) shall only be connected to ground at the main power service disconnect switch and at the controller cabinet with UPS and generator.
i. Equipment grounding conductors shall be brought to the main switch, but shall be grounded at both secondary and main switches.
7. Payment for the above work shall be incidental to Item 625 No. 4 AWG 600 Volt Distribution Cable, As Per Plan

ITEM 625 NO. 4 AWG 600 VOLT DISTRIBUTION CABLE, AS PER PLAN

A green colored, insulated, cable shall be used for the ground wire (GND) where indicated. This GND cable shall be separate from the ground rod wire, but shall be connected to the same grounding bolt used for the ground rod wire attachment at the pole. The GND cable shall be tagged as "GND SYS" at all pole locations & control cabinets.

Payment shall be as per Item 625.

ITEM 625 PULL BOX, (BY TYPE), (BY SIZE), AS PER PLAN

Pull boxes shall have the word "TRAFFIC" on the lid. All pull boxes within or adjacent to sidewalk shall be flush with the walk.

Payment shall be as per Item 625.

ITEM 625 CONDUIT, (BY SIZE), 725.052 (SCH 80), JACKED OR DRILLED, AS PER PLAN

Conduit placed under proposed roadways shall have a minimum overall depth of 24 inches and a minimum depth of 12 inches under the final pavement subgrade whichever is deeper. This item of work shall include exposing all water line and gas line crossings and restoration of disturbed areas to original condition.

Payment shall be as Item 625.

ITEM 625 LUMINAIRE, POST TOP, LED, 120 VOLT, AS PER PLAN

The Contractor shall furnish and install Granville Series - Leaf Style Casting Luminaires GVD3-P50-30K-MVOLT-CLF-GL3-BK-CL as manufactured by Halophane, 214 Oakwood Avenue, Newark, Ohio 43055. Luminaires shall be IES type III, 99 watt LED and operate on 120volt/60 hertz circuits. Luminaires shall be mounted as per manufacturer's instructions. Each intersection shall run off a single photoelectric cell. The luminaire and all mounting hardware shall be painted to match the signal support.

Measurement and payment shall be as per item 625.

ITEM 630 SIGN, SINGLE FACED, STREET NAME, AS PER PLAN

Under this item of work the Contractor shall furnish and install LED internally illuminated street name signs, as manufactured by National Sign and Signal, South Armstrong Road, Battle Creek, Michigan 49015 (616) 957-2817, or approved equal. Internally illuminated signs shall be supplied with all the hardware for mounting below the specified mast arm.

The sign shall be constructed of cast and extruded aluminum components welded to provide a weatherproof housing suitable for outdoor use. Drip rails shall overhang the sign face to prevent water from entering the electrical housing. Drain holes shall be provided in the bottom of the housing. Each door shall be secured to the housing by a full length, open, stainless steel hinge on the door and three-quarter turn air lock fasteners. Neoprene gasketing shall form a watertight seal between the door and the housing. The exterior of the sign housing shall be primed and coated to match the signal supports. Surface preparation shall be as per the paint manufacturer's recommendations. Prior to painting the Contractor shall submit paint samples to the Engineer and Sunbury officials for review and comment.

All sign components shall be accessible when one door is open and shall not require removal of the sign for access or maintenance operations. LED's shall be mounted to a white aluminum panel and have a conformal coating applied for protection. LED drivers shall be housed inside the sign enclosure and shall not be visible from the exterior.

Signs shall operate on 120 volt/60 hertz power and shall be capable of continuous operation over a range in temperatures from -35 degrees to 140 degrees Fahrenheit. A wire entrance junction box shall be supplied with the sign and shall mount inside the sign enclosure. Signs shall not have individual photocells.

The sign face shall be constructed of shatterproof, translucent polycarbonate material. The sign face and legend shall conform to the requirements of the Ohio Manual of Uniform Traffic Control Devices. The background color shall be translucent green with twelve inch white upper case and nine inch lower case letters. A clear coat shall be applied to all sign faces to prevent fading and protect the surface.

Mounting hardware shall be provided to rigid mount the sign to the mast arm. Mounting brackets shall be adjustable to allow the sign to be plumb. All mounting hardware shall be manufactured of heat treated aluminum bar stock, stainless steel, or galvanized steel of adequate design and strength for its intended use. All hardware, including stainless steel cables if used, shall be painted as per this specification to match the mast arm and support.

Payment shall be made at the contract unit price bid per each sign including all labor, materials, connections, and appurtenances complete, in place, tested and accepted.

ITEM 630 SIGNING, MISC.: TRAFFIC SIGNAL SIGNS

When used, the contractor shall install W3-3-36 "SIGNAL AHEAD" signs with W16-15P-24 "NEW" supplemental placards and posts if required just prior to the signal being placed on flash. The "NEW SIGNAL" and "SIGNAL AHEAD" signs shall be covered until the signal is placed on regular operation at which time the contractor shall uncover them.

The Contractor shall remove these items 3 weeks after they are uncovered. At the same time, the Contractor shall return any permanent sign, which has been temporarily shifted in order to accommodate the "signal operation changed" sign, to the location specified in the plans. Failure to remove these items and relocate permanent signs shall result in the city removing them, along with relocating permanent signs, and billing the contractor for all costs involved. All removed material shall be considered forfeited to the city.

Mounting hardware and posts shall be incidental to this bid item.

Payment shall be per the unit price bid to install and remove all necessary signs at each intersection.



Jordan C. Steele, Date 9/28/25

For Power Distribution and Grounding Only on Sheets 51 through 64.

CALCULATED MSJ CHECKED DMW/ADS

TRAFFIC SIGNAL NOTES

CHESHIRE ROAD RELOCATION

ITEM 632 VEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, WITH BACKPLATE (BLACK), AS PER PLAN

ITEM 632 VEHICULAR SIGNAL HEAD, (LED), 4-SECTION, 12" LENS, 1-WAY, WITH BACKPLATE (BLACK), AS PER PLAN
In addition to the requirements of C&MS 632 and 732, the following requirements shall apply:

1. Signal heads and visors shall be constructed of black polycarbonate plastic with visors as specified and meet ITE specifications.
2. Proper exterior colors shall be obtained by use of colored plastic material rather than painting.
3. All upper signal support hardware and piping up to and including the wire inlet fitting shall be ferrous metal.
4. The entrance fitting shall be of the tri-stud design with serrated rings in order to achieve positive locking.
5. All signal heads shall be rigidly mounted to the mast arm with the (color) module located in front of the mast arm.
6. Aluminum backplates shall be in accordance with the C&MS and include a fluorescent yellow reflective border.
7. 4-Section vehicular signal heads shall provide for Flashing Yellow Arrow operation for permitted left turns as detailed in the plans.

ITEM 632 COVERING OF VEHICULAR SIGNAL HEAD, AS PER PLAN

Cover vehicular signal heads if erected at intersections where traffic is maintained before energizing the signals. Use a sturdy opaque covering material specifically made for use with traffic signals, and ensure that the color of the cover is different than the signal head, top or beige, so that it is clear to drivers the heads are covered, not dark. Use a method of covering to cover attachment and materials approved by the Engineer. Covers are to be free of text, pictures, or any type of advertising. Maintain covers and remove them when directed by the Engineer.

Payment shall be per Item 632.

ITEM 632 PEDESTRIAN SIGNAL HEAD, AS PER PLAN

In addition to the requirements of C&MS 632 and 732 the following shall apply:

1. Signal heads and visors shall be constructed of black polycarbonate plastic to match the signal supports and meet ITE specifications.
2. Proper exterior colors shall be obtained by use of colored plastic material rather than painting.
3. Pipe, spacers and fittings constructed of polycarbonate plastic may be used in lieu of galvanized steel or aluminum.
4. The pedestrian signal head shall be of the LED countdown type.
5. New attachment hardware and fittings shall be used.
6. The light emitting diode (LED) modules shall meet the requirements of C&MS 732.04-C. The Contractor shall provide, in writing, the LED manufacturer name, serial number, part number, description of lamp, and date of manufacture for all led units that are to be used in the signal head prior to installation, for acceptance and warranty purposes.
7. Pedestrian signal heads shall display international symbols in lieu of the words "WALK" and "DON'T WALK".
8. A clear shatterproof, lens shall cover the face of the signal head and be of sufficient strength to protect the components from roadside hazards and vandalism.

Payment shall be per Item 632 and including all labor, equipment, materials and new attachment hardware.

ITEM 632 ACCESSIBLE PEDESTRIAN PUSHBUTTON, AS PER PLAN

The exterior pushbutton housing shall be black to match the signal supports. The pushbutton shall be a Polara IN52 accessible pushbutton system. The pushbuttons shall be programmed by the manufacturer to provide verbal commands regarding the state of the pedestrian signal. Upon pressing the pushbutton and when the pedestrian signal head displays "Don't Walk", a verbal command shall be provided, "Wait, Wait to Cross (Street Name)". When the pedestrian phase is active and the pedestrian signal head displays "Walk", a verbal command shall be provided, "Walk sign on cross (Street Name)". The pushbutton shall be rated for medium or heavy duty usage and have a barrier type of lug terminal for attachment of the field cable. A clear bead of silicone sealant shall be applied to the top of the pushbutton housing (1 inch each side of top center) against the pole to prevent water from entering the back of the pushbutton housing. One aluminum sign shall be supplied with each pushbutton. The bottom of the sign shall be mounted just above the top of the pushbutton. Mount the center of the pushbutton 42" above the pedestrian pathway surface. The signs shall be as detailed in these plans.

The back of all signs, mounting hardware, and support assemblies mounted on either signal supports or pedestal supports shall be coated to match its respective support. Finish requirements shall be in accordance with that listed for the support, pedestal, or light pole used for attachment.

Payment shall be as per Item 632.

ITEM 632 COMBINATION SIGNAL SUPPORT, TYPE TC-81.22, (BY SIZE), AS PER PLAN

The signal support foundations shall be excavated or vacuum excavated to test for conflicts prior to shop drawings approval. Foundation voids shall be temporary backfilled, securely covered, or the foundations restamped and supports are manufactured. If utility conflicts are encountered during excavation, the Engineer shall be notified. Modifications to the foundation location may be required. Vacuum excavation, and foundation location adjustments shall be incidental to Item 632 Signal Support Foundation, As Per Plan.

The vertical pole upright shall be tapered and 16 sharp fluted galvanized steel montube. The fluting of the upright shall not start until the point which is above the height of the decorative clam shell base to ensure a smooth fit between the pole and the top of the decorative base. The mast arms shall be a tapered, round and smooth with an upsweep as illustrated on the pole details. Arms 40 feet or less shall be of one piece construction. Arms greater than 40 feet can be of two piece construction. Mast arms greater than 59' in length (or as directed by the project engineer) shall be fitted with a vibration damping device to reduce fatigue and increase structure longevity. Arms shall be clamp on style.

A decorative two-piece clam shell base shall be provided as specified herein with each signal support. The decorative clam shell base shall have a maximum height of 40 inches. A decorative base of 42 inches will not be accepted. Decorative bases shall be elastomeric urethane or cast aluminum and similar to:

- Valmont Enduro, Huntington series.
- Union Metal: Columbian Family
- Millerbernd: Augusta Series
- Or approved equal.

Combination signal supports shall include a 120 V outlet with duplex GFI receptacle, orientated and attached as shown in the details included within.

1-1/2 inch bind hole couplings shall be provided at the orientation and height specified on the signal support, orientation and fabrication data sheet for pedestrian signal head mounting.

All signal cables shall be run inside the poles. Standard Drawing TC-83.10 is hereby modified to prohibit the use of external conduit risers for signal cabling, or any other use.

The Contractor shall provide all necessary attachments or connections to the poles. Additional wiring holes in the pole shall be drilled, reamed, or hole sawed. Flame cutting (oxyacetylene or electrical arc) shall be avoided. All cut edges or other defects in the zinc coating shall be cleaned and covered with two coats of zinc rich repair paint matching the factory finish. Brackets and appurtenances shall be securely attached with stainless steel screws of sufficient size for the intended loading. Stainless steel banding shall not be used unless specifically authorized by these plans and specifications or directed by the Engineer. All banding, where used, shall be factory painted to match the signal supports.

Design Criteria

In addition to the requirements of item 632, signal support structures shall be designed and constructed by the supplier to support the loads caused by the signs, signals, and other equipment that the plan requires the contractor to install. The use of standard ODOT design designations such as those described on ODOT standard drawings, and any details provided in this plan, are intended to promote uniformity of design and are not warranted to be structurally adequate. To the maximum extent practicable, the Contractor and supplier shall provide a structurally adequate support that utilizes standard ODOT anchor bolt sizing and spacing.

In addition to provisions of the ODOT C&MS, furnish and install signal poles as specified in the plans. The signal support designer shall provide drawings of each signal support with structural aspects of the design and materials in compliance with the AASHTO, LRFDLS-1, Design Criteria as listed on ODOT standard construction drawing TC-81.22, note No. 33 shall apply.

Submit, to the Engineer prior to incorporation: signal support drawings and shop drawings, which identify and describe the manufactured signal support and signal support item which is being incorporated into the construction. The signal support drawings and shop drawings shall each be reviewed, sealed, stamped, and dated by an Ohio registered Professional Engineer.

The mast arm support pole assembly, and the mast arm assembly shall be made by the same manufacturer and designed and sold as a unit.

All pre-drilled holes for all items shall be deburred and free of all sharp edges. All outside welds shall be rolled or ground smooth. All inside welds shall be void of sharp edges. Any structural fastener (3/4" or greater) shall be galvanized per ASTM 153 and shall be made of high strength carbon steel. Any other fastener (less than 3/4") shall be stainless steel. All visible fasteners shall match the coating of the structure.

Finish

Poles and arms, including base and arm clamp, bolt covers, handholes, and wire entrances, shall be hot dipped galvanized when applicable after fabrication in accordance with 711.02. All visible elements of the signal support, and any other parts required to be coated or painted, shall be galvanized and then powder-coated or painted. All painting shall be performed under controlled environmental conditions, and in accordance with the paint manufacturer's recommendations pertaining to surface preparation, material handling, and application.

All structural steel products shall be galvanized on the interior and exterior surfaces as per ASTM A123. The exterior surface of all structural steel and aluminum products shall be properly prepared for the application of an exterior coating. The coating color on both steel and aluminum products shall match each other. It is the responsibility of the contractor to ensure that both product manufacturers match coating colors.

The inside of each pole assembly and pole accessories shall be galvanized. The inside area formed by the gussets, pole and pole flange plate shall also be galvanized to protect the area from corrosion. It is to be noted that some type of opening shall be required to galvanize the gusset area. This opening shall not hamper the structural integrity of the flange assembly. External surfaces of the mast arm structure shall be galvanized.

Pole structure surfaces shall be prepared for galvanizing by using a solvent cleaning method to remove dirt, oil, dust, grease and any other contaminant that will reduce the adhesion of the galvanizing. Mill scale, rust or any other foreign material not removed by solvents shall be removed using mechanical methods. (Per steel structures painting council: SSPC-VIS-1-67) manufacturer recommended abrasive techniques shall be used to provide the proper profile for adherence to ASTM A123 galvanic coating. The galvanizing shall be properly prepared so the powder coating or paint will adhere to the galvanized support. The structure shall then be coated or painted as per plan. Each coating layer shall be properly cured before the application of the next coat. Dry thickness measurements for steel shall conform to section SSPC-PA2-82, steel structures painting council.

All exterior surfaces of the mast arm pole shaft assembly, mast arm assembly, two-piece base, all clamps, all wire entrances, all clevis hangers, all handhole covers, brackets, luminaire arms, finials, pole and arm caps and traffic pedestal structures shall have a coating applied to them.

Exterior surfaces of all bolt and screw fasteners, washers, nuts, and other attachment hardware shall have a coating applied to them. Fastener threads shall not be clogged with coating material.

The following shall apply for all the items above:

1. The top finish coat of paint shall be Black, per the Aerospace Material Specifications Standard 595 (ASM) color number 17038. Paint samples shall be submitted with the signal support shop drawings for review.
2. Be applied over properly prepared galvanizing material on steel products and over properly prepared aluminum for aluminum products and have a minimum 5-year repair warranty of coating delamination, blistering, or corrosion. All coating systems shall meet the minimum requirements of ODOT Supplemental Specification 916 - standard performance based paint processes for light poles, sign supports and traffic supports for wet-coat systems.
3. All coated or painted items shall be shipped in a manner to minimize damage in transit. Surfaces should be protected from padding, by wrapping in cardboard, by spiral wrapping with wax paper, by crating, by a combination of methods or by any other method selected by the manufacturer which will ensure delivery of undamaged materials. Materials damaged in transit shall be repaired or replaced. All costs associated with correcting damaged material shall be borne by the contractor.

Anchor bolts shall be oriented as required by the plans however the formed top of the support foundation shall be oriented square to the adjacent sidewalk where applicable.

Payment

This item of work shall be measured as each complete signal support, in place in essentially a vertical position under full plan loading. All labor, equipment, and materials necessary to pickup, transport, store, erect, adjust, and repair the signal support and anchor bolts shall be included for payment in the bid item. Payment shall be as per Item 632.

ITEM 632 PEDESTRIAN PEDESTAL, 11', AS PER PLAN

Pedestals shall be as per City of Columbus Standard Construction Drawing 4106, except that it shall be 11' in height. The decorative base shall be of similar appearance to the base requirements of Item 632 Combination Signal Support, Type TC-81.22, (By Size), As Per Plan.

All exterior surfaces shall be coated in accordance with the finish section requirements of Item 632 Combination Signal Support, Type TC-81.21 (BY SIZE), As Per Plan. All signal cables shall be run inside the poles. Standard Drawing TC-83.10 is hereby modified to prohibit the use of external conduit risers for signal cabling, or any other use.

Payment shall be as per Item 632.

ITEM 632 SIGNAL SUPPORT FOUNDATION, AS PER PLAN

The formed top of the anchor base pole foundation shall be oriented parallel to the sidewalk or back-of-curb or edge-of-pavement as shown on the signal plans. The top of the foundation shall be flush with any adjacent sidewalk or concrete area. A minimum of two conduit ells, used or unused, shall be installed in each pole foundation.

Contractor shall vacuum excavate proposed mast arm foundation locations prior to installation. The signal support mast arm foundations shall be excavated or vacuum excavated to test for conflicts prior to shop drawings approval. Foundation voids shall be temporary backfilled, securely covered, or the foundations installed while signal supports are manufactured. If utility conflicts are encountered during excavation, the Engineer shall be notified. Modifications to the foundation location may be required. Vacuum excavation, and foundation location adjustments shall be incidental to this item of work.

Payment shall be as per Item 632.

ITEM 632 PEDESTAL FOUNDATION, AS PER PLAN

The formed top of the pole foundation shall be oriented parallel to the sidewalk or back-of-curb or edge-of-pavement as shown on the signal plans. The top of the foundation shall be flush with any adjacent sidewalk or concrete area. A minimum of two conduit ells, used or unused, shall be installed in each pedestal foundation.

Prior to ordering the pedestals, the Contractor shall contact OUPS to have all the utilities located in the field to insure there are no conflicts with utilities. If there are issues, the Project Engineer shall provide guidance as to the relocation of the support poles.

Breakaway anchor bolts shall be used for pedestal foundations.

Payment shall be as per Item 632 and will be at the contract unit price and will be full compensation for all labor, materials, tools, equipment and other incidentals necessary for each support foundation furnished, in place, complete and accepted.

ITEM 632 POWER SERVICE, AS PER PLAN

ITEM 632 POWER CABLE, 3-CONDUCTOR, CU, NO. 6 AWG, AS PER PLAN
Power cable shall be provided as per 632.23 details of the control cabinet and the top-in location noted in the plan. When the power cable is in place and two weeks prior to the time that electrical power will be required, the Contractor shall contact Consolidated Electric Cooperative which will make the electrical service connection. Under no circumstances shall the Contractor connect power cable into the power company's circuits. A common photo electric relay and controller with HAND/OFF/AUTO switch shall control all street lighting mounted on signal supports. The photo electric relay shall be located on the signal pole closest to the controller. The voltage supplied shall be 120/240 volts, 120 volts per circuit with one circuit for traffic signals and one circuit for street lighting on the signal supports. Power cable conductors shall be copper. The neutral of the power cable shall only be grounded in the main power service disconnect switch and at controller cabinet with UPS and generator.

Provide an Arc Flash Hazard Warning sign on the outside front door of the enclosure in accordance with the current National Electrical Code paragraph 110.16.

Provide an available fault current sign on the outside of the front door of the power service disconnect switch at the controller cabinet in accordance with the National Electrical Code paragraph 110.24.

Measurement and payment shall be as per Item 632.

ITEM 809 ATC CONTROLLER, AS PER PLAN

ITEM 633 CABINET, TYPE TS-2, AS PER PLAN

The controller shall be an Econote Cobalt with EOS software in an integrated controller/UPS cabinet. The following shall be provided under this item of work:

1. The controller shall include a minimum of two Ethernet ports and data keys.
2. Back-up protection shall be enabled
3. A minimum of two 120 volt GFI receptacles shall be installed in the cabinet
4. An outlet receptacle and box shall be installed in the cabinet to protect network equipment from an imbalance flow current from the hot to the neutral. The outlet shall be a NEMA duplex 5-15 receptacle, rated at 15 amps (minimum) at 120 VAC. The outlet shall meet or exceed federal specifications and UL 498 standards and shall be rated as weather-resistant. The receptacle shall be installed within a metallic, single gang electrical box with a cover plate. The electrical box shall be standard depth (nominally 2 - 1/8 in.) And shall be UL-listed. The outlet shall be installed inside the cabinet along one of the side walls and shall be wired from the same circuit breaker as the other outlets, unless otherwise directed by the Engineer.
5. A surge suppression device shall be installed in the cabinet to provide protected power outlets to network equipment. The surge suppression device shall be securely mounted in the cabinet in a method approved by the Engineer. The surge suppression device installed shall have 6 NEMA 5-15 outlets and shall be capable of being plugged into a standard 5-15 outlet. The output current of the surge suppression device shall be 15 amps. The surge suppression device shall have an energy handling rating of 1280 joules, UL 1499 let through rating of 330 volts, and surge current rating of 50,000 amps.
6. The controller cabinet shall have two ground rods installed six feet apart. The rods shall be connected by a ground wire jumper that is welded to each rod.
7. Signal head field wiring shall be labeled with phase number, direction, movement type, and color (red, yellow, green, yellow arrow, green arrow) or pedestrian movement.

CALCULATED
MS/J
CHECKED
DMM/ADS

TRAFFIC SIGNAL NOTES

CHESHIRE ROAD RELOCATION

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8. The Contractor shall provide the cabinet wiring diagram/plans in PDF format to the City of Sunbury. Include a set of back panel prints in the cabinet for the City's maintenance crews.
9. The cabinet shall provide for an integral battery back-up compartment with a separate door.
10. The terminal facilities shall be TS2 configuration.
11. A slide out laptop shelf.
12. Interior, undershell LED cabinet lighting, including a minimum of 2 panels of 6 high-intensity LEDs each and a door activated switch. The LED panels shall be mounted in locations to maximize light on the cabinet equipment.
13. Cabinet shall be furnished with a cabinet riser.
14. A gooseneck/adjustable light fixture with an LED lamp. The adjustable light fixture shall be mounted on the upper right side of the controller cabinet. Provide an Arc Flash Warning sign on the outside front door of the cabinet in accordance with the National Electrical Code paragraph 110.16.
15. Provide an Arc Flash Warning sign on the outside front door of the cabinet in accordance with the National Electrical Code paragraph 110.16.
16. Cabinet shall include wiring from uninterruptible power supply to signal cabinet to provide alarms for on battery, two hour timer, low battery and off battery in signal cabinet.
17. Auxiliary power shall charge batteries.
18. Provide a 20 amp circuit breaker ahead of the street lighting circuit contactor pole.
19. Provide a 15 amp circuit breaker ahead of the receptacle circuit contactor pole.
20. Provide a multi-pole contactor (for the luminaires and receptacles) and two HOA switches (one for the luminaires and the other for the receptacles).
21. The exterior of the cabinet shall be unpainted bare aluminum.

Payment shall be made at the unit price bid for each CMS Item 633, Controller Unit, Type TS2/A2 with Cabinet, Type TS2, As Per Plan complete and in place, all connections tested and accepted.

ITEM 633 UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN

This item of work shall consist of furnishing and installing a Battery Backup UPS system capable of maintaining power at an intersection during a utility power failure. The intersection shall remain fully operational for a period of a minimum of four (4) hours under a full-load. The system shall be self-contained including the UPS hardware, the required number of batteries and its own ventilated enclosure. The controller and battery back-up cabinet shall appear as one cabinet from the outside with two internal compartments accessed by separate doors.

The Battery Back-up System shall meet the requirements of ODOT CMS 633 and 733 and be listed in the ODOT TAP approved products list. The Battery Back-up System shall be MultiLink EP1100 with Safety Automatic Transfer Switch (SATS), Battery Balancer and include NEMA L1430-F1 Inlet for Generator hook-up.

The UPS shall have the following features and capabilities:

1. The unit shall be rated for a maximum output power of 1000VA (or 700Watts)
2. Output Voltage regulation of +/- 5%
3. Allow for Hot Swappable Batteries and temperature-compensated charging to maximize battery life.
4. Intelligent Boost Operation for Brownout Protection - providing a stable output during low voltage conditions
5. Include safety approvals to UL-1778, CSA-107.1 and EN60950
6. The UPS shall be configurable and monitored by a front panel Ethernet Port.
7. Include a means to switch the intersection from full-operation to flashing operation after 2-hours of run-time. This is to conserve battery operation during an extended utility power outage.
8. Include standard form C relay contacts to trigger an alarm within the controller assembly, informing a technician the system is operating on battery back up. Alarms to the traffic controller shall include On Battery, Low Battery, 2 Hour Run Time and Flash.
9. Include a Safety Automatic Transfer Switch (SATS), separate from the UPS to connect UPS power when the utility line is unqualified. The ATS will also allow for Generator Hook-up and hot swapping of the UPS
10. The Batteries used in the UPS shall use Absorbent Glass Mat (AGM) technology and shall be of the Valve Regulated Lead Acid (VRLA) construction.
11. Four (4) batteries shall be supplied with the UPS system. Each battery shall be 12VDC and be rated for 105Ahr, minimum.
12. All batteries shall be placed on battery heater mats in the enclosure.
13. Include optional battery balancer device that regulates the batteries and optimizes performance.
14. The UPS System shall be installed in the UPS Compartment of the traffic cabinet and shall include a vent, fan and thermostat.
15. Be equipped with remote communication capabilities to monitor battery status.

All connections, wiring, attachment hardware and miscellaneous materials for full operation of the UPS system shall be included in the unit price bid for this item. Payment shall be per CMS Item 633, Uninterruptible Power Supply (UPS), 1000 Watt, As Per Plan and shall be made at the unit price bid per each, complete, in place, tested and accepted.

ITEM 633 CABINET FOUNDATION, AS PER PLAN

The cabinet foundation shall be sized to accommodate the UPS cabinet on the same foundation. Provide a spare 3" conduit ell per TC-83.20.

Payment will be made at the unit bid price for each CMS Item 633, Cabinet Foundation, As Per Plan.

ITEM 633 CONTROLLER WORK PAD, AS PER PLAN

The controller work pad shall be sized to accommodate the UPS cabinet on the same work pad.

Payment will be made at the unit bid price for each CMS Item 633, Controller Work Pad, As Per Plan.

ITEM 633 COMMUNICATION SYSTEM, AS PER PLAN

Under this item of work the Contractor shall furnish and install communication equipment at the proposed traffic signal installation. The Communications equipment shall conform to ODOT CMS Specification 633.15. The model supplied shall be Applied Information Field Monitoring Unit FMU/2 with Glance Integration.

Antennas for both the cellular and 900MHZ radios shall be mounted to the top of the traffic signal cabinet enclosure and sealed to prevent water intrusion into the cabinet.

This item shall include a 10-year connectivity and service plan with each Field Monitoring Unit. Each device shall also include a 10-year full replacement warranty. The warranty shall be extendable to match the duration of the connectivity plan.

Wiring, couplers, modifications of the cabinet facilities, and all other devices, whether or not specifically itemized on the plan or these notes shall be provided and installed as required to render the system completely operational.

Payment for Item 633 Communication System, As Per Plan shall be made at the contract unit price for each, complete and in place including all required cabinet hardware, mounting brackets, cables, connections tested and accepted.

ITEM 633 CONTROLLER ITEM, MISC.: GPS TIME SYNCHRONIZATION

Under this item of work the Contractor shall furnish and install GPS time synchronization equipment in the traffic signal cabinet. The time-based GPS unit shall be connected to the proposed traffic signal controller.

This item of work shall consist of providing and installing all parts and equipment, complete with all wiring, connections and miscellaneous items and accessories needed to render the GPS time synchronization fully operational, in place, tested and accepted.

Payment for Item 633 Controller Item, Misc.: GPS Time Synchronization shall be per each unit.

ITEM 816 VIDEO DETECTION SYSTEM, AS PER PLAN

Under this item of work the Contractor shall furnish and install a complete video vehicle detection system consisting of the following:

- Currux Vision Smart City ITS Platform video detection system.
- Two (2) AXIS M4318-PLVE Panoramic Cameras.

The video vehicle detection system shall monitor vehicles on a roadway via processing of video images and provide detector outputs to the traffic signal controller. Each system shall outfit one entire intersection and shall include two image sensors (cameras) and all necessary cables.

Wiring, couplers, modification of the cabinet facilities, and all other devices, whether or not specifically itemized herein, shall be provided and installed as required to render the video system completely operational in compliance with the Ohio Department of Transportation Standards, Supplemental Specifications 816 and 907 and the Manufacturer's recommendations. All major components of the video vehicle detection system shall be supplied by the same manufacturer or manufacturers as listed above as a complete system, ready to operate.

All devices except the image sensors shall be housed in the controller cabinet. All devices shall be mounted so all cable connections are accessible and all device doors can be fully opened for servicing. The video vehicle detection system shall be capable of being controlled via a remote supervisor computer. The user shall be able to create edit, store and delete detection zones from either the supervisor computer or at the remote intersection location.

The detection sensors shall be housed in an environmental enclosure that is waterproof and dust tight to NEMA-4 specifications.

All exposed exterior surfaces of the video sensors, mounting hardware, and related items shall be painted to match the signal supports. Detection units shall be mounted on candy cane style bracket, which shall be coated to match the signal support and be considered incidental to this item of work.

All video detection wiring shall exit the mast arm within one foot of the riser pipe location and provide for a drip loop. The camera wiring shall also be located inside the riser pipe. Video detection wiring shall not be run out of the end of the mast arm. In addition to the requirements outlined above payment for the Video Detection System, As Per Plan shall include the following:

All devices, wiring, and cabinet modifications required to make the video vehicle detection system fully operational shall be provided, installed and paid for under this item of work. Payment for all of the above will be made at the contract unit price bid, including all labor, materials, and appurtenances for each complete video vehicle detection system at one intersection, installed, tested, and accepted.

ITEM 809 CCTV IP-CAMERA SYSTEM, DOME-TYPE

In addition to the requirements of Item 809 the following shall apply:

- The Contractor shall furnish and install a Hanwha PNM-C34404RQPZ quad view camera with PTZ.
- Pole mount adapter - coated to match the color of the signal support.

Wiring, couplers, modifications of the cabinet facilities, and all other devices, whether or not specifically itemized on the plan or these notes shall be provided and installed as required to render the system completely operational. All mounting hardware, connections, and cables shall be considered incidental to this item of work. All visible elements of the system except the bubble lens shall be coated to match the color of the traffic signal supports. Power shall be obtained from the adjacent signal cabinet and all power cable shall be furnished and installed under this item of work.

Payment shall be per each, per Item 809.

ITEM SPECIAL - POLE MOUNTED RECEPTACLE, 120V, DUPLEX GFI

A receptacle shall be included on each pole where detailed herein. Ground fault interrupting (GFI) receptacles shall be 20 ampere specification grade, weather resistant type with weatherproof in-use housing and cover per NEC 406.9(B). Receptacles shall run off of the same photocell as the luminaires.

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CHECKED
DBM/JDS

TRAFFIC SIGNAL NOTES

CHESHIRE ROAD RELOCATION

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Traffic Signal Estimate of Quantities

Item	Quantity	Unit	Item Description
625	120	LF	Conduit Encased, 2", 725.051
625	290	LF	Conduit Encased, 3", 725.051
625	206	LF	Conduit, 2", 725.051
625	324	LF	Conduit, 3", 725.051
625	418	LF	Conduit, 3", 725.052 (SCH 80), Jacked or Drilled, As Per Plan
625	2	EA	Pull Box, 725.06, 18", As Per Plan
625	4	EA	Pull Box, 725.08, 24", As Per Plan
625	418	LF	Trench
625	418	LF	Plastic Caution Tape
625	10	EA	Ground Rod
625	4	EA	Luminaire, LED, 120 Volt, As Per Plan
625	4	EA	Connection, Fused Pull-Apart, Type II
625	4	EA	Connection, Unfused Pull-Apart, Type III
625	1316	LF	No. 8 AWG, 600 Volt Distribution Cable (Combination Lighting)
625	707	LF	No. 4 AWG, 600 Volt Distribution Cable, As Per Plan (Grounding and Bonding)
625	300	LF	Pole and Bracket Cable, No. 10 AWG, 600 Volt
630	4	EA	Sign, Single Faced, Street Name, As Per Plan
630	1	EA	Signage, Misc.: Traffic Signal Signs
632	9	EA	Vehicular Signal Head, (LED), 3-Section, 12" Lens, 1-Way, With Backplate (Black), As Per Plan
632	2	EA	Vehicular Signal Head, (LED), 4-Section, 12" Lens, 1-Way, With Backplate (Black), As Per Plan
632	8	EA	Pedestrian Signal Head, As Per Plan
632	11	EA	Covering of Vehicular Signal Head, As Per Plan
632	8	EA	Covering of Pedestrian Signal Head
632	8	EA	Accessible Pedestrian Pushbutton, As Per Plan
632	1	EA	Combination Signal Support, Type TC-81.22, Design No. 12, As Per Plan
632	1	EA	Combination Signal Support, Type TC-81.22, Design No. 13, As Per Plan
632	2	EA	Combination Signal Support, Type TC-81.22, Design No. 14, As Per Plan
632	4	EA	Signal Support Foundation, As Per Plan
632	4	EA	Pedestal Foundation, As Per Plan
632	4	EA	Pedestrian Pedestal, 11", As Per Plan
632	2654	LF	Signal Cable, 7 Conductor, No. 14 AWG
632	1834	LF	Signal Cable, 3 Conductor, No. 14 AWG
632	1618	LF	Loop Detector Lead-In Cable
632	1	EA	Conduit Riser, 2" Diameter, SCH 80
632	158	LF	Power Cable, 3-Conductor, Cu, No. 6 AWG, As Per Plan
632	1	EA	Power Service, As Per Plan
633	1	EA	Cabinet, Type TS-2, As Per Plan
633	1	EA	Cabinet Foundation, As Per Plan
633	1	EA	Controller Work Pad, As Per Plan
633	1	EA	Uninterruptible Power Supply (UPS), 1000 Watt, As Per Plan
633	1	EA	Communication System, As Per Plan
633	1	EA	Controller Item, Misc.: GPS Time Synchronization
809	1	EA	ATC Controller, As Per Plan
809	1	EA	CCTV IP-Camera System, Dome-Type
816	1	EA	Video Detection System, As Per Plan
Special	4	EA	Pole Mounted Receptacle, 120V, Duplex GFI

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TRAFFIC SIGNAL QUANTITIES

CHESHIRE ROAD RELOCATION

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MATERIAL SPECIFICATIONS FOR BBS GENERATOR POWER PANEL EQUIPMENT

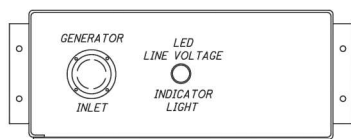
GENERATOR INLET - The inlet shall be 30 amp, 125/250V, locking, four (4) wire grounding and meet the NEMA configuration number L14-30-P 30A 125/250V specification. The inlet shall be a Hubbell catalog #2715.

LINE VOLTAGE GENERATOR SWITCH - The switch shall be 30 amp, 125/250V AC, two (2) pole, three (3) position (On, Off, On). The switch shall be a Hubbell catalog #1388.

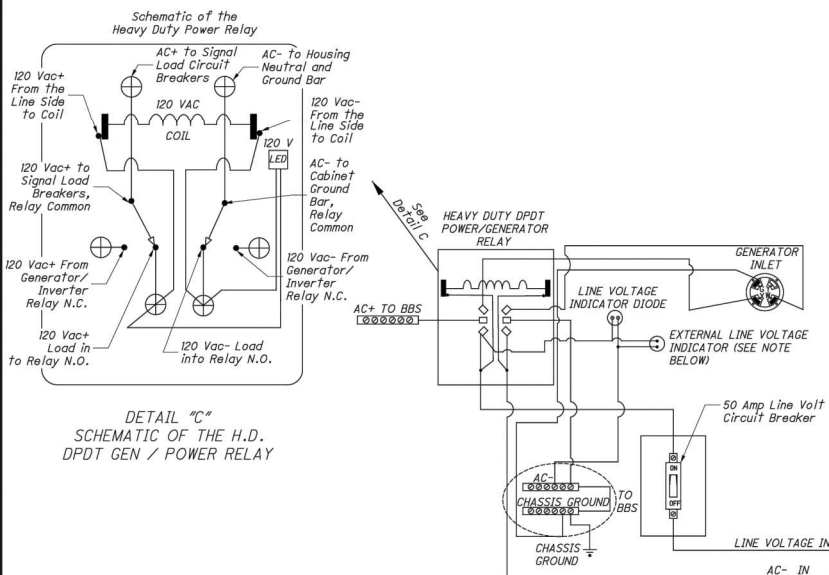
LINE VOLTAGE INDICATOR LIGHT - The indicator light shall be 125V AC light emitting diode with a red lens.

LINE VOLTAGE CIRCUIT BREAKER - The circuit breaker shall be single pole single throw and a minimum of 30 amps. The amperage shall be increased to accommodate greater loads, if necessary. The gauge of the power cable shall be of proper size per N.E.C.

EXTERNAL LINE VOLTAGE INDICATOR LIGHT - The indicator light shall be a 1" waterproof NEMA 4X or IP66 LED lamp with a green lens.



FRONT VIEW OF GENERATOR POWER PANEL

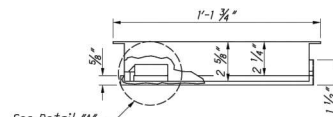


DETAIL "C"
SCHEMATIC OF THE H.D.
DPDT GEN / POWER RELAY

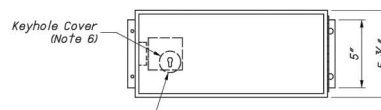
ELECTRICAL HOOKUP DETAIL FOR THE BBS GENERATOR POWER PANEL

NOTE: EXTERNAL LINE VOLTAGE INDICATOR LIGHT required when called for in the plans.
EXTERNAL LINE VOLTAGE INDICATOR LIGHT shall be located on the enclosure exterior for visibility from the adjacent roadway when all cabinet, and generator panel doors are closed.

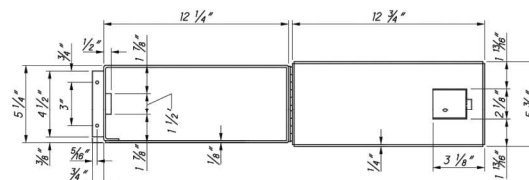
GENERATOR POWER PANEL ENCLOSURE



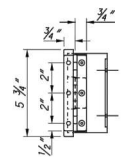
TOP VIEW



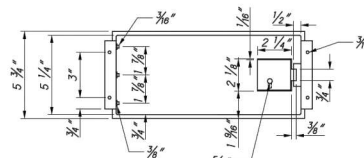
FRONT VIEW CLOSED DOOR



FRONT VIEW OPEN DOOR



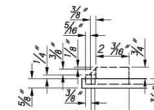
RIGHT SIDE VIEW
CLOSED DOOR



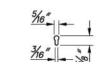
BACK VIEW CLOSED DOOR

NOTES:

1. The enclosure shall be constructed of 1/8" thick aluminum.
2. The lock shall be the standard police door type, keyed with the standard Flasher door skeleton key.
3. The door shall be sealed with a foam rubber gasket to prevent moisture from entering the enclosure.
4. The enclosure shall be mounted onto the outside of the controller cabinet with non-accessible bolts and sealed with a high quality silicon caulk at all surfaces touching the cabinet.
5. The hinge shall be of stainless steel or equivalent corrosion-resistant material.
6. Keyhole shall be covered with a movable circular aluminum or brass cover with top pivot pin.



DETAIL "A"



DETAIL "B"

THIS DRAWING REPLACES PIS 203012 DATED 04-20-2012.

OFFICE OF
ROADWAY
ENGINEERING

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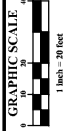
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TRAFFIC SIGNAL DETAILS

CHESHIRE ROAD RELOCATION

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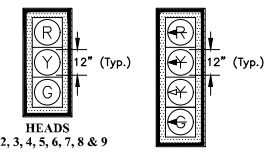
CALCULATED MSV CHECKED DRW/LDS

TRAFFIC SIGNAL PLAN

CHESHIRE ROAD RELOCATION

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PROPOSED VEHICULAR TRAFFIC SIGNAL HEAD CONFIGURATION WITH BACK PLATES



➔ SOLID YELLOW ARROW
➔ FLASHING YELLOW ARROW

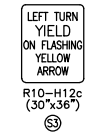
PROPOSED PEDESTRIAN SIGNAL HEAD CONFIGURATION



ILLUMINATED STREET NAME SIGNS SINGLE SIDED



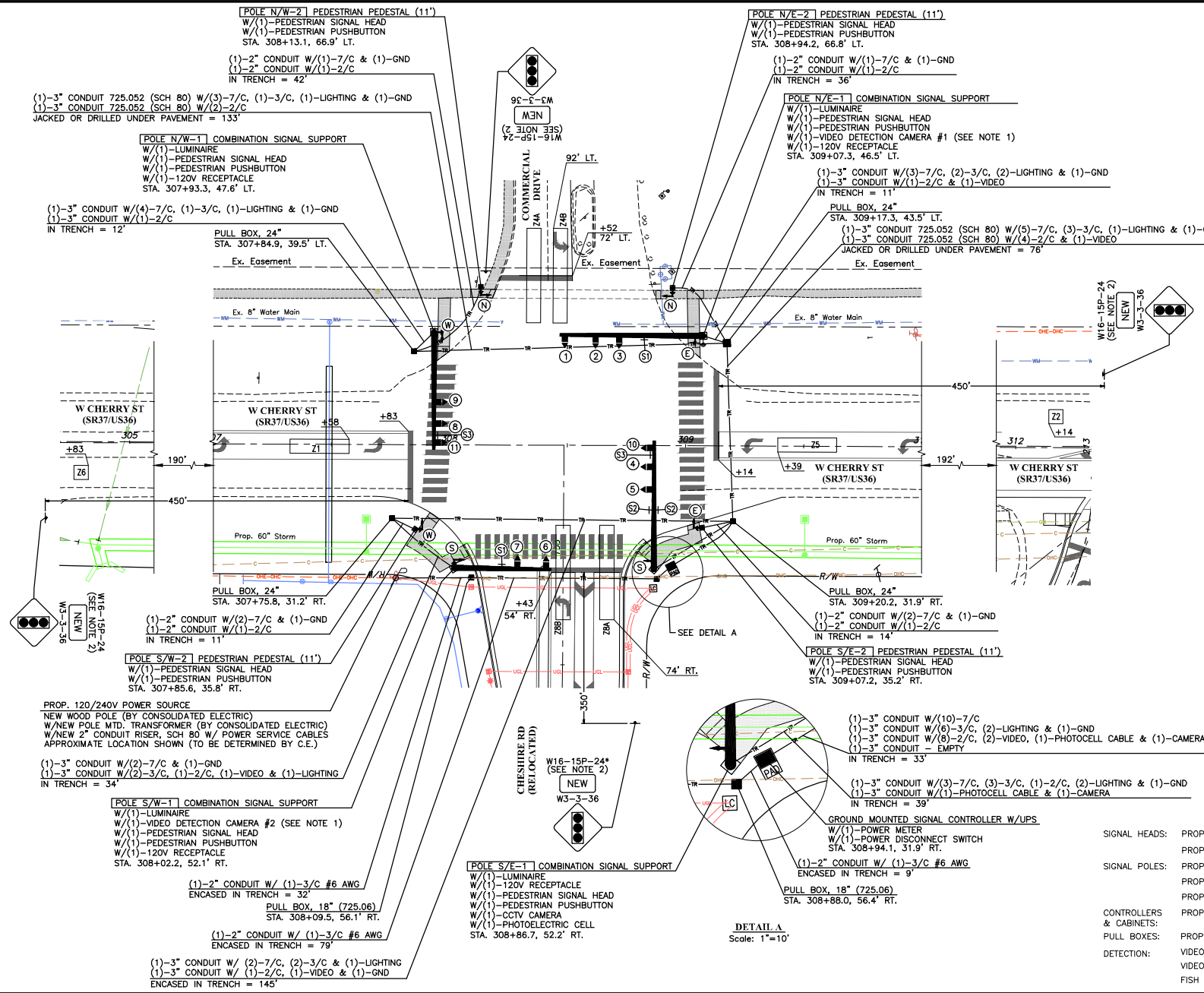
PROPOSED ARM MOUNTED SIGN



- NOTES
- The Contractor shall coordinate with the video detection supplier to orient the camera per the manufacturer's recommendations.
 - The Contractor shall install the sign at the approximate location shown. The signs shall be mounted to an existing pole if available and shall be no more than 12' from the edge of pavement. The Contractor shall ensure the sign is placed such that adequate sight distance is provided.

LEGEND

- SIGNAL HEADS: PROP. VEHICULAR
 PROP. PEDESTRIAN
 SIGNAL POLES: PROP. ANCHOR/STRAIN POLE
 PROP. PEDESTAL EX. PEDESTAL PUSHBUTTON
 PROP. MASTARM
 PROP. CABINET W/PAD
 CONTROLLERS & CABINETS: PROP. CABINET W/PAD
 PULL BOXES: PROP. PULL BOX
 DETECTION: VIDEO DETECTION ZONE
 VIDEO DETECTION UNIT
 FISH EYE CAMERA



SIGNAL TIMING CHART

INTERSECTION: CHESHIRE RD/ W CHERRY ST (SR 37/US RT 36)	
MAINTAINING AGENCY: VILLAGE OF SUNBURY	
START UP	DUAL ENTRY: YES PHASES: 2 + 6 4 + 8
START IN: RED FLASH	REST IN RED: INACTIVE RING 1 - RING 2 -
TIME FOR FLASH OR ALL RED: 10	OVERLAP
FIRST PHASE(S): 2 + 6	E G - -
COLOR DISPLAYED: GREEN	PHASES
	ø1+ø2 ø5+ø6 - -
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.
INTERSECTION MOVEMENT (PHASE)	1 2 3 4 5 6 7 8
DIRECTION	EBLT WB - SB WBLT EB - NB
MINIMUM GREEN (INITIAL) (SEC.)	10 15 - 10 10 15 - 10
ADDED INITIAL *(SEC./ACTUATION)	- - - - -
MAXIMUM INITIAL (SEC.)	- - - - -
PASSAGE TIME (PRESET GAP) (SEC.)	3.6 2.0 - 3.6 3.6 2.0 - 3.6
TIME BEFORE REDUCTION *(SEC.)	- - - - -
MINIMUM GAP *(SEC.)	- - - - -
TIME TO REDUCE *(SEC.)	- - - - -
MAXIMUM GREEN I (SEC.)	15 60 - 30 15 60 - 30
MAXIMUM GREEN II (SEC.)	- - - - -
YELLOW CHANGE (SEC.)	3.9 4.8 - 4.1 3.9 4.8 - 4.1
FYA DELAY (SEC.)	- 2 - - -
ALL RED CLEARANCE (SEC.)	1.8 1.0 - 2.0 1.8 1.0 - 2.0
WALK (SEC.)	- 7 - - -
PEDESTRIAN CLEARANCE (SEC.)	- 17 - - 16 - 15 - 17
RECALL	MAXIMUM (ON/OFF) OFF OFF - OFF OFF OFF - OFF
	MINIMUM (ON/OFF) OFF ON - OFF OFF ON - OFF
	PEDESTRIAN (ON/OFF) OFF ON - OFF OFF ON - OFF
MEMORY	(ON/OFF) OFF ON - OFF OFF ON - OFF

*VOLUME DENSITY CONTROLS

Traffic Signal Coordination Data will be furnished by the City to the Contractor at the time of construction. It shall be the Contractor's responsibility to implement the proposed timing plans of the intersections listed below in the table and implement timing adjustments as the intersections of Cherry Street/SR 3 and Cherry Street/Miller Drive as directed. All timing data modifications/implementation shall be considered incidental to Item 809 ATC Controller, As Per Plan.

Cherry Street at Kintner Parkway

PATTERN	CYCLE	OFFSET	FULL DEMAND PHASE SPLIT (SEC.)							
AM PEAK	60	-	-	-	-	-	-	-	-	-
PM PEAK	60	-	-	-	-	-	-	-	-	-
OFF PEAK	60	-	-	-	-	-	-	-	-	-

Cherry Street at Cheshire Road

PATTERN	CYCLE	OFFSET	FULL DEMAND PHASE SPLIT (SEC.)							
AM PEAK	60	-	-	-	-	-	-	-	-	-
PM PEAK	60	-	-	-	-	-	-	-	-	-
OFF PEAK	60	-	-	-	-	-	-	-	-	-

- AM PEAK = 6-9AM
- PM PEAK = 3-7PM
- OFF PEAK = 9AM-3PM
- FREE OPERATION SHOULD BE IMPLEMENTED BETWEEN 7PM-10PM
- FLASH OPERATION SHOULD BE IMPLEMENTED BETWEEN 10PM-6AM
- COORDINATED PHASE REFERENCED TO BEGINNING OF YELLOW ON THRU PHASE FOR S. MILLER DRIVE.

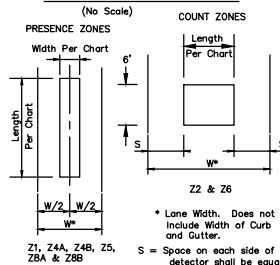
FIELD WIRING HOOK-UP CHART

SIGNAL HEAD #	INDICATION	FIELD TERMINAL	FLASH	SIGNAL HEAD #	INDICATION	FIELD TERMINAL	FLASH
1 (NB)	R	ø8 R	R	10 (EBLT)	R	ø6 R	R
	Y	ø8 Y			ø6 Y		
	G	ø8 G			ø6 G		
2 (NB)	R	ø8 R	R	11 (WBLT)	R	ø2 R	R
	Y	ø8 Y			ø2 Y		
	G	ø8 G			ø2 G		
3 (NB)	R	ø8 R	R	PEDESTRIAN MOVEMENTS			
	Y	ø8 Y		ø2 G			
	G	ø8 G					
4 (EB)	R	ø6 R	R	S	WALK	G ø6-W	OFF
	Y	ø6 Y		DON'T WALK	R ø6-DW		
	G	ø6 G					
5 (EB)	R	ø6 R	R	E	WALK	G ø6-W	OFF
	Y	ø6 Y		DON'T WALK	R ø6-DW		
	G	ø6 G					
6 (SB)	R	ø4 R	R	N	WALK	G ø2-W	OFF
	Y	ø4 Y		DON'T WALK	R ø2-DW		
	G	ø4 G					
7 (SB)	R	ø4 R	R	W	WALK	G ø4-W	OFF
	Y	ø4 Y		DON'T WALK	R ø4-DW		
	G	ø4 G					
8 (WB)	R	ø2 R	R				
	Y	ø2 Y					
	G	ø2 G					
9 (WB)	R	ø2 R	R				
	Y	ø2 Y					
	G	ø2 G					

VIDEO DETECTION ASSIGNMENTS

DETECTOR	CAMERA/UNIT	PHASE	DETECTION ZONE SIZE	PRESENCE	DELAY DATA	
					DELAY (sec.)	INHIBIT DELAY DURING GREEN ø
Z1	2	1	6'x25'	X	3	1
Z2	1	2	6'x6'	-	-	-
Z4A	2	4	6'x40'	X	12	4
Z4B	2	4	6'x40'	X	3	4
Z5	1	5	6'x25'	X	3	5
Z6	2	6	6'x6'	-	-	-
Z8A	1	8	6'x40'	X	12	8
Z8B	1	8	6'x40'	X	3	8

VEHICLE DETECTOR DETAILS

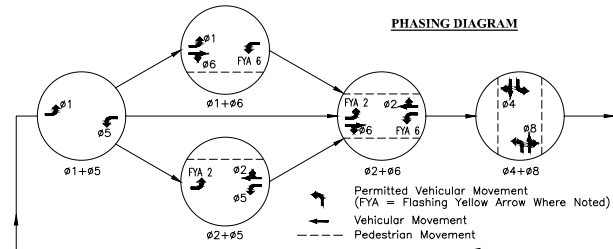


OLE = PPLT/FYA (PROTECTED TURN = PHASE 1, PERMISSIVE THROUGH = PHASE 2)
 OLG = PPLT/FYA (PROTECTED TURN = PHASE 5, PERMISSIVE THROUGH = PHASE 6)
 OLE FYA OUTPUT MODE = LS 13 YEL PED
 OLE FYA OUTPUT MODE = LS 15 YEL PED

Controller Notes:

1. Configure MMU for mode H FYA operation.
2. When applicable, simulated loads shall be applied on all unused ped load switch walk outputs for all FYA signals. The simulated load shall be easily removed if/when walk signals are installed.
3. FYA DELAY programmed to FYA overlap that corresponds to permissive through movement in timing chart.

PHASING DIAGRAM



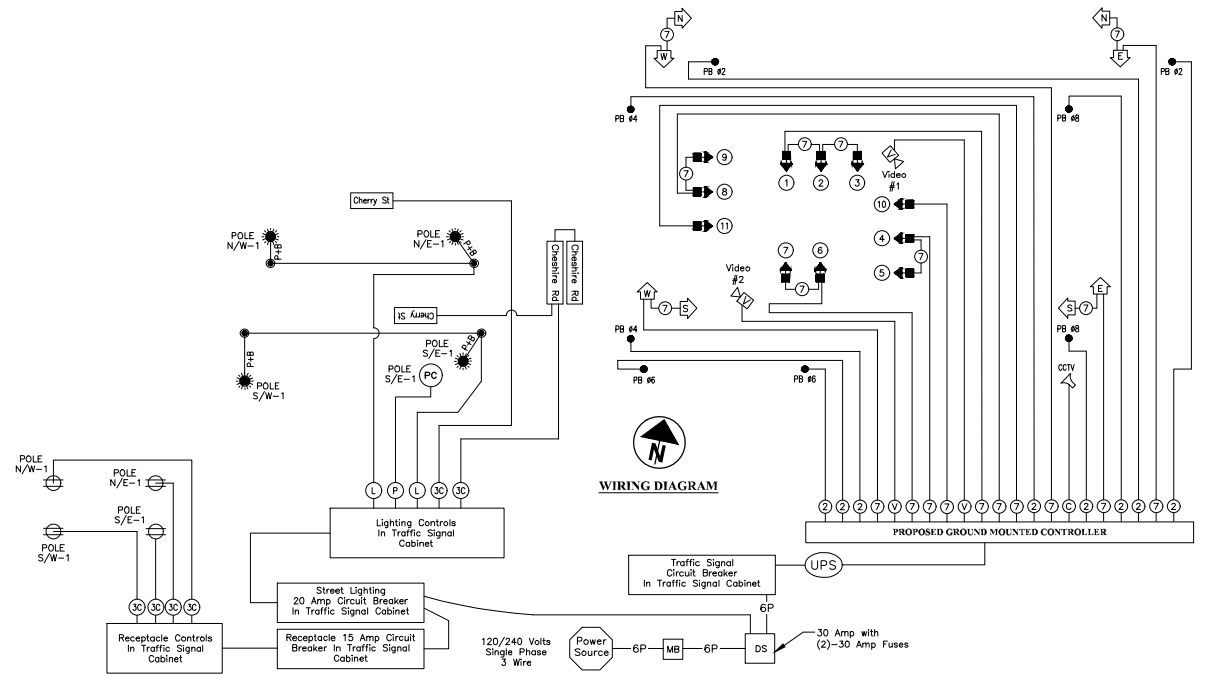
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TRAFFIC SIGNAL DETAILS

CHESHIRE ROAD RELOCATION

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WIRING DIAGRAM LEGEND

- | | | | |
|--|--|--|---|
| | Vehicular Signal Head | | Connection of Distribution Cable to Pole and Bracket Cable Connector Kit Fused and Unfused. |
| | 3/C #6 AWG (Power) | | Past Top Luminaire |
| | Pedestrian Signal Head | | Photocell |
| | Pedestrian Pushbutton | | GND Ground Wire |
| | Power Source
120/240 Volt
Single Phase Power | | #10 AWG Pole and Bracket Cable |
| | Power Meter | | 120V Ple Mtd. Receptacle |
| | Distribution Cable #6 AWG (2 Wire) | | Video Detection Unit |
| | Main Power Disconnect Switch | | CCTV Camera |
| | 3 Conductor, #14 AWG | | Camera Cable |
| | Illuminated Street Name Sign | | Video Detection Cable |
| | Photocell Cable | | Signal Cable, 7 Conductor, #14 AWG |
| | | | Signal Cable, 2 Conductor, #14 AWG (Lead-In Cable) |

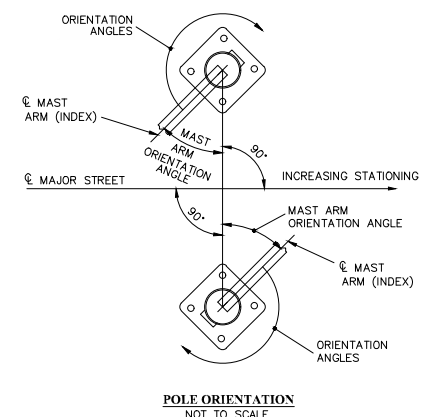
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TRAFFIC SIGNAL DETAILS

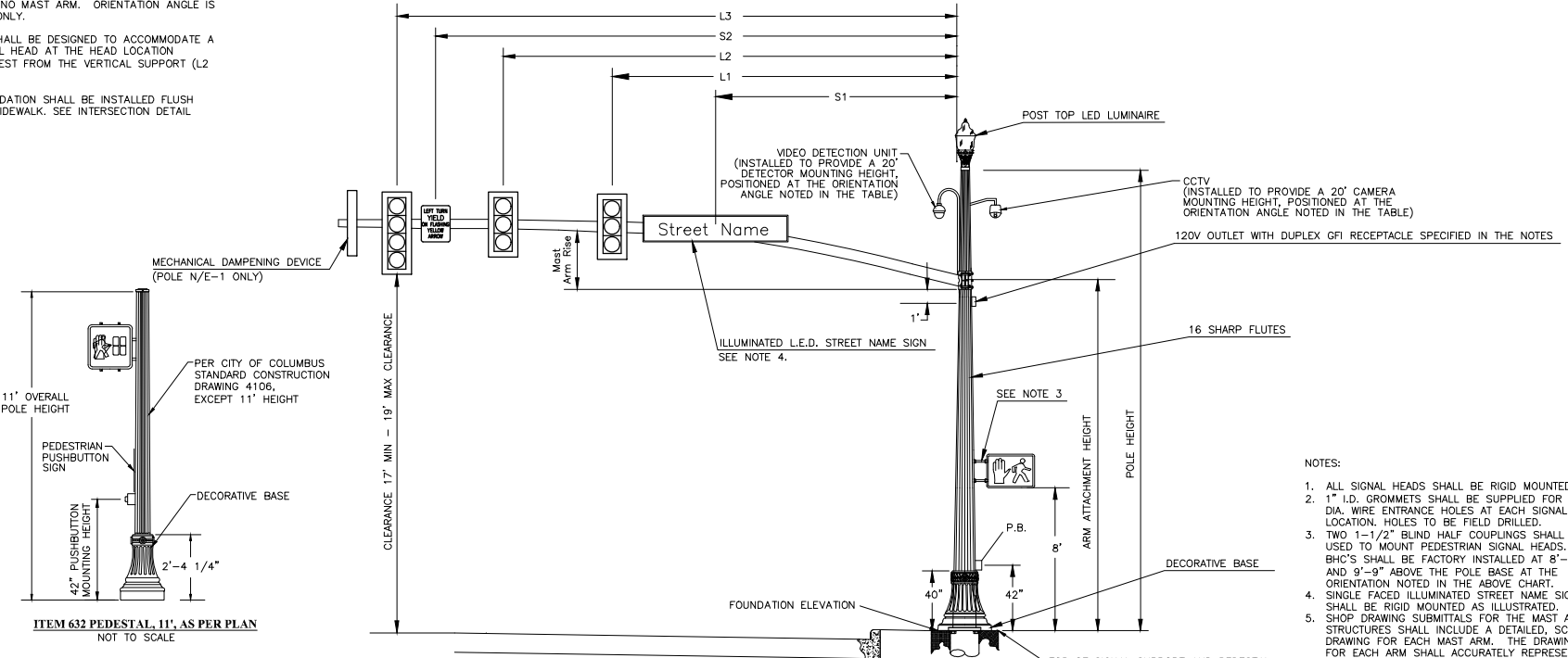
CHESHIRE ROAD RELOCATION

SIGNAL SUPPORT AND MAST ARM TABLE

SUPPORT NO.	DESIGN TYPE	DESIGN NO.	SIGNAL SUPPORT DETAILS									ORIENTATION ANGLES FROM MAST ARM								FIELD ORIENTATION DATA		
			POLE HEIGHT	ARM ATTACHMENT HEIGHT	MAST ARM LENGTH	MAST ARM RISE	L1	L2 **	L3 **	S2	ILLUMINATED STREET NAME SIGN (SNS) (S1)	PEDESTRIAN SIGNAL	120V OUTLET WITH RECEPTACLE	PEDESTRIAN BUTTON	LUMINAIRE ANGLE	CCTV CAMERA	VIDEO DETECTION CAMERA	HANDHOLE	INDEX LINE ANGLE MAST ARM	ANCHOR BOLT REF. LINE	POLE FOUNDATION ELEVATION	
			FT	FT	FT	FT	FT	FT	FT	FT	FT	DEG	DEG	DEG	DEG	DEG	DEG	DEG	DEG	DEG	DEG	
N/W-1	TC-81.22	DESIGN 13	23.5	17	49.5	5	29.5	38.5	46.5	43.5	-	270°	180°	270°	0°	-	-	180°	0°	90°	***	
N/W-2	COC-4106	N/A	11	-	-	-	-	-	-	-	-	90°	-	90°	-	-	-	180°	270° *	180°	***	
N/E-1	TC-81.22	DESIGN 14	22	15.5	61.5	5	35.5	45.5	58.5	-	25	0°	180°	0°	270°	-	270°	180°	90°	180°	***	
N/E-2	COC-4106	N/A	11	-	-	-	-	-	-	-	-	270°	-	270°	-	-	-	180°	90° *	180°	***	
S/E-1	TC-81.22	DESIGN 14	23	16.5	54.5	5	34	43.5	51.5	48.5	25 / 25	0°	180°	0°	0°	0°	-	180°	0°	90°	***	
S/E-2	COC-4106	N/A	11	-	-	-	-	-	-	-	-	7°	-	0°	-	-	-	180°	262° *	352°	***	
S/W-1	TC-81.22	DESIGN 12	22.5	16	42	5	27	39	-	-	20	270°	180°	270°	270°	-	270°	180°	90°	180°	***	
S/W-2	COC-4106	N/A	11	-	-	-	-	-	-	-	-	327°	-	327°	-	-	-	180°	127° *	217°	***	



NOTES:
 * PEDESTAL HAS NO MAST ARM. ORIENTATION ANGLE IS FOR REFERENCE ONLY.
 ** MAST ARMS SHALL BE DESIGNED TO ACCOMMODATE A 5-SECTION SIGNAL HEAD AT THE HEAD LOCATION PROPOSED FURTHEST FROM THE VERTICAL SUPPORT (L2 OR L3).
 *** TOP OF FOUNDATION SHALL BE INSTALLED FLUSH WITH ADJACENT SIDEWALK. SEE INTERSECTION DETAIL SHEETS.



SIGNAL ELEVATION DETAIL
NOT TO SCALE

- NOTES:**
- ALL SIGNAL HEADS SHALL BE RIGID MOUNTED.
 - 1" I.D. GROMMETS SHALL BE SUPPLIED FOR 1.38" DIA. WIRE ENTRANCE HOLES AT EACH SIGNAL LOCATION. HOLES TO BE FIELD DRILLED.
 - TWO 1-1/2" BLIND HALF COUPLINGS SHALL BE USED TO MOUNT PEDESTRIAN SIGNAL HEADS. THE BHC'S SHALL BE FACTORY INSTALLED AT 8'-0" AND 9'-9" ABOVE THE POLE BASE AT THE ORIENTATION NOTED IN THE ABOVE CHART.
 - SINGLE FACED ILLUMINATED STREET NAME SIGNS SHALL BE RIGID MOUNTED AS ILLUSTRATED.
 - SHOP DRAWING SUBMITTALS FOR THE MAST ARM STRUCTURES SHALL INCLUDE A DETAILED, SCALED DRAWING FOR EACH MAST ARM. THE DRAWING FOR EACH ARM SHALL ACCURATELY REPRESENT THE INITIAL ARM ATTACHMENT ANGLE, ARM CURVATURE RADIUS AND THE RISE AFTER LOADING.

CALCULATED
MSJ
CHECKED
DRM/ADS

POLE ORIENTATION

CHESHIRE ROAD RELOCATION

60
65

C:\Users\johnd\OneDrive\Documents\2025\Cheshire Road Relocation\2025\Cheshire Road Relocation.dwg, 10/17/2025 2:29 PM
 User: johnd, Plot: 2025-09-25 10:11:00, Plot Device: HP DesignJet 2550, Plot Style: 2025-09-25 10:11:00, Plot Size: 11x17, Plot Orientation: Landscape

LIGHTING NOTES

- THE CITY OF COLUMBUS, OHIO, DIVISION OF POWER, CONSTRUCTION AND MATERIAL SPECIFICATIONS ARE HEREBY INCORPORATED INTO THIS SET OF PLANS. THE DIVISION OF POWER STANDARD SPECIFICATIONS ARE OBTAINED AT: [HTTPS://WWW.COLUMBUS.GOV/TEMPLATES/DETAIL.ASP?ID=2147503308](https://www.columbus.gov/templates/DETAIL.ASP?ID=2147503308). THE MIS NUMBERS LISTED IN THE STREET LIGHTING QUANTITIES ARE REFERENCES TO SPECIFICATIONS PUBLISHED BY THE DIVISION OF POWER WHICH SHALL GOVERN ALL WORK RELATING TO THE INSTALLATION OF ROADWAY LIGHTING EQUIPMENT & APPARATUS FOR THIS PROJECT.
- RESIDENTIAL OR COMMERCIAL ZONING DISTRICTS: ALL PUBLIC STREET LIGHTING WITHIN A PUBLIC STREET RIGHT-OF-WAY IN A RESIDENTIAL OR COMMERCIAL ZONING DISTRICT SHALL BE MAIN STREET LIGHTING, A114BK FOR THE LIGHT POLES / LAMP POSTS, AND THE GENERAL ELECTRIC, EVOLVE EPAM FOR THE LUMINAIRES, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER AND/OR CITY COUNCIL. THE LIGHT POLE/LAMP POST MUST BE FOURTEEN (14) FEET IN LENGTH TO PROVIDE A LIGHT CENTER MOUNTING HEIGHT OF APPROXIMATELY FIFTEEN (15) FEET SIX (6) INCHES. THE POLE SHALL HAVE A THREE (3) INCH TOP MOUNT THE POST TOP LUMINAIRE. THE POLE SHALL FEATURE CAST ALUMINUM BASES, A TAPERED SPUN ALUMINUM SHAFT, WHICH IS STRONG AND LIGHT WEIGHT FOR EASE OF INSTALLATION AND REQUIRE LITTLE MAINTENANCE AND BE DAMAGE RESISTANT IN AREAS WHERE LAWN AND SNOW EQUIPMENT COMES IN DIRECT CONTACT WITH THE BASE. FOR MULTIPLE LUMINAIRE ORIENTATION A THREE (3) INCH DIAMETER LANTERN TOP SHALL BE STANDARD. THE FINISH OF THE BASE/SHAFT SHALL BE WITH SHERWIN WILLIAMS POLANE POLYURETHANE FINISH OVER SHERWIN WILLIAMS INDUSTRIAL PRIMER OR APPROVED EQUIVALENT. THE BASE AND SHAFT SHALL BE FADE AND CHEMICAL RESISTANT AND COLOR IS TO BE BLACK. THE POLES SHALL BE FURNISHED WITH A HAND HOLE AND REMOVABLE COVER. THE REMOVABLE COVER SHALL HAVE VANDAL RESISTANT FASTENERS TO PREVENT ACCESS. THE SEAL AND COVER SHALL HAVE THE SAME COLOR AND TEXTURE AS THE POLE AND BE EQUIPPED WITH A VANDAL RESISTANT FASTENER. THE POLE SHALL HAVE A NINE (9) AND ONE-EIGHTH (1/8) INCH DIAMETER BOLT CIRCLE WITH A FOUR (4) BOLT PATTERN.
- FOUNDATIONS FOR ROADWAY LIGHT STANDARDS SHALL BE LOCATED 2'-0" BEHIND CURBS AT LOCATIONS INDICATED IN THE PLANS. WHERE UTILITIES INTERFERE WITH LIGHT POLE LOCATIONS, THE FOUNDATION MAY BE MOVED A MAXIMUM OF 3'-0" FROM LONGITUDINAL LOCATION SHOWN.
- PROVIDE A VACANT ELL OUT OF LIGHT POLE FOUNDATIONS IN WHICH CIRCUIT TERMINATES FOR FUTURE EXTENSION OF THE LIGHTING SYSTEM.
- PULL BOXES SHALL BE LOCATED APPROXIMATELY WHERE SHOWN ON PLANS WITH EXACT LOCATION OF EACH PULL BOX TO BE DETERMINED AFTER CAREFUL CONSIDERATION HAS BEEN GIVEN TO THE LOCATION OF UTILITIES. THE QUANTITY OF PULL BOXES MAY VARY DEPENDING UPON THE ACTUAL SITE CONDITIONS AND LOCATION OF UTILITIES.
- ALL LIGHTING WIRING TO BE #14 XHHW W/ #8 AWG XHHW GND. CONDUIT TO BE MINIMUM OF 2" SCH 40 PVC.
- ALL SPLICES TO BE MADE WITH WATERPROOF CONNECTORS PER MIS-404 AND 1001.14.
- PROVIDE 5' OF COILED SPARE WIRE IN EACH PULL BOX FOR EACH LUMINAIRE.
- EACH POLE TO HAVE IN-LINE WATERPROOF 5 AMP FUSE LOCATED AT POLE BASE, ACCESSIBLE AT HANDHOLE.
- PULL BOXES SHALL COMPLY WITH NEC 314.30. PULL BOXES SHALL BE SET ON MINIMUM OF 6" CRUSHED STONE, SHALL BE RATED FOR TIER 15, SHALL BE PRECAST MONOLITHIC POLYMER CONCRETE AND SHALL CONFORM TO ANSI/SCTE 77 "SPECIFICATIONS FOR UNDERGROUND ENCLOSURE INTEGRITY". COVERS TO BE IN ACCORDANCE WITH ASTM C1026.
- STREET TREES SHALL NOT BE INSTALLED WITHIN TEN FEET OF A PROPOSED STREET LIGHT POLE (MEASURED FROM TREE TRUNK TO CENTER OF LIGHT POLE).

**ITEM 1001. LUMINAIRE, 59W, 240V, WITH HOUSE SIDE SHIELD, AS PER PLAN
ITEM 1001. LUMINAIRE, 59W, 240V, WITH HOUSE SIDE SHIELD, AS PER PLAN, FURNISH ONLY**

BASE BID SHALL INCLUDE THE INDICATED NUMBER OF 59 WATT LED POST TOP LUMINAIRES, ACORN TYPE. LUMINAIRE SHALL BE COATED BLACK TO MATCH THE COLOR OF THE LIGHT POLE
AN EXTRA LUMINAIRE IS TO BE PROVIDED FOR EVERY 25, OR FRACTION THEREOF, SUPPLIED; MINIMUM ONE (1) EXTRA TO BE SUPPLIED ON ALL PROJECTS.
FEATURES: CAST ALUMINUM CONSTRUCTION - POST TOP FITTERS (PAINTED TO MATCH LIGHT POLE) WILL ACCEPT EXTERIOR HARDWARE - GLOBES ARE ACRYLIC - MUST BE EQUAL TO GE EVOLVE, EPAM-01-0-10-B-30-1-1H-A-X-X-BLCK-R.
THE LED LUMINAIRE SHALL BE 59 WATT, 3000 CCT, AND HAVE A TYPE 3 DISTRIBUTION WITH HOUSE SIDE SHIELDS INSTALLED. HOUSE SIDE SHIELD, SHALL BE PART #11SHS-EPAM01 (93150929).
SHOP DRAWINGS SHALL BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL PRIOR TO ORDERING. CITY ENGINEER RESERVES THE RIGHT TO MODIFY THE REQUIREMENTS AT ANY TIME PRIOR TO SHOP DRAWING APPROVAL.
THE CONTRACTOR SHALL COORDINATE WITH THE CITY FOR DELIVERY AND DROP OFF OF THE SPARE PARTS (FURNISH ONLY). PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH LUMINAIRE FURNISHED AND INSTALLED BY THE CONTRACTOR.

**ITEM 1001. LUMINAIRE, 77W, 240V, WITH HOUSE SIDE SHIELD, AS PER PLAN
ITEM 1001. LUMINAIRE, 77W, 240V, WITH HOUSE SIDE SHIELD, AS PER PLAN, FURNISH ONLY**

LUMINAIRE SHALL BE MANUFACTURED BY GENERAL ELECTRIC, EVOLVE EPAM-01, 77W LED ACORN.
BASE BID SHALL INCLUDE THE INDICATED NUMBER OF 77 WATT LED POST TOP LUMINAIRES, ACORN TYPE.
LUMINAIRE SHALL BE COATED BLACK TO MATCH THE COLOR OF THE LIGHT POLE
AN EXTRA LUMINAIRE IS TO BE PROVIDED FOR EVERY 25, OR FRACTION THEREOF, SUPPLIED; MINIMUM ONE (1) EXTRA TO BE SUPPLIED ON ALL PROJECTS.
FEATURES: CAST ALUMINUM CONSTRUCTION - POST TOP FITTERS (PAINTED TO MATCH LIGHT POLE) WILL ACCEPT EXTERIOR HARDWARE - GLOBES ARE ACRYLIC - MUST BE EQUAL TO GE EVOLVE, EPAM-01-0-10-B-30-1-1H-A-X-X-BLCK-R.
THE LED LUMINAIRE SHALL BE 77 WATT, 3000 CCT, AND HAVE A TYPE 3 DISTRIBUTION WITH HOUSE SIDE SHIELDS INSTALLED. HOUSE SIDE SHIELD, SHALL BE PART #11SHS-EPAM01 (93150929).
SHOP DRAWINGS SHALL BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL PRIOR TO ORDERING. CITY ENGINEER RESERVES THE RIGHT TO MODIFY THE REQUIREMENTS AT ANY TIME PRIOR TO SHOP DRAWING APPROVAL.
THE CONTRACTOR SHALL COORDINATE WITH THE CITY FOR DELIVERY AND DROP OFF OF THE SPARE PARTS (FURNISH ONLY). PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH LUMINAIRE FURNISHED AND INSTALLED BY THE CONTRACTOR.

**ITEM 1001. LIGHT POLE, AS PER PLAN
ITEM 1001. LIGHT POLE, AS PER PLAN, FURNISH ONLY**

LIGHT POLE SHALL BE MANUFACTURED BY MAIN STREET LIGHTING, LANCASTER II SERIES #AA114, 14' HEIGHT. POLE SHALL BE FURNISHED WITH FLAG POLE BRACKET.
BASE BID SHALL INCLUDE THE INDICATED NUMBER OF ALUMINUM POLES AS HERINAFTER DESCRIBED.
AN EXTRA POLE IS TO BE PROVIDED FOR EVERY 25, OR FRACTION THEREOF, SUPPLIED; MINIMUM ONE (1) EXTRA TO BE SUPPLIED ON ALL PROJECTS.
MATERIAL: THE POLE SHALL BE 14' IN LENGTH TO PROVIDE A LIGHT CENTER MOUNTING HEIGHT OF APPROXIMATELY 15'6". THE POLE SHALL HAVE A 3" TENON TO MOUNT THE POST TOP LUMINAIRE.
FEATURES: CAST ALUMINUM BASE, TAPERED SPUN ALUMINUM SHAFT - STRONG & LIGHTWEIGHT FOR EASE OF INSTALLATION - REQUIRES LITTLE MAINTENANCE - DAMAGE RESISTANT IN AREAS WHERE LAWN/SNOW EQUIPMENT COMES IN DIRECT CONTACT WITH BASE - MULTIPLE LUMINAIRE ORIENTATION - 3" DIAMETER LANTERN TENON STANDARD.
FINISH: BASE/SHAFT FINISHED WITH SUPERIOR QUALITY ACRYLIC URETHANE FINISH OVER PRIMED ALUMINUM SURFACE THAT IS ULTRA VIOLET RESISTANT & IMPERVIOUS TO CANINE URINE. BASE AND SHAFT ARE FADE AND CHEMICAL RESISTANT AND COLOR SHALL BE BLACK, PER THE AEROSPACE MATERIAL SPECIFICATIONS STANDARD 595 (ASM) COLOR NUMBER 17038.
OTHER FEATURES: THE POLE SHALL BE FURNISHED WITH A HAND HOLE AND REMOVABLE COVER AND SEAL & COVER SHALL HAVE THE SAME COLOR AND TEXTURE AS THE POLE AND BE EQUIPPED WITH A VANDAL RESISTANT FASTENER. POLE SHALL HAVE A 9" DIAMETER BOLT CIRCLE WITH A 4 BOLT PATTERN. PROVIDE A GROUNDING LUG.
THE ALUMINUM POLE SHALL BE EQUAL IN QUALITY, DESIGN & PERFORMANCE TO MAIN STREET LIGHTING (#AA114-BK).
INSTALLATION: THE POLES SHALL BE PLACED ON THE AUGER TYPE FOUNDATION SIMILAR IN QUALITY, DESIGN & PERFORMANCE TO THE A. B. CHANCE CO. (#112-0679).
THE CONTRACTOR SHALL COORDINATE WITH THE CITY FOR DELIVERY AND DROP OFF OF THE SPARE PARTS (FURNISH ONLY). PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH LIGHT POLE FURNISHED AND INSTALLED BY THE CONTRACTOR.

ITEM 1001. STREET LIGHTING CIRCUIT CABLE, AS PER PLAN

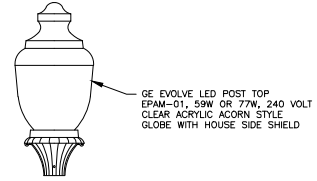
IN ADDITION TO THE REQUIREMENTS OF MIS-404, STREET LIGHTING CIRCUIT CABLE FURNISHED FOR THIS PROJECT SHALL CONTAIN TWO (2) #4 AWG, 600 VOLT XHHW CABLES AND ONE (1) #8 AWG, 600 VOLT XHHW GROUND CABLE.
PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH CIRCUIT FOOT OF CABLE.
ITEM 1001. POLE TO BE WIRED, AS PER PLAN
STREET LIGHT POLES SHALL BE WIRED AS PER MIS-501, EXCEPT THAT IT SHALL BE DESIGNED FOR A THREE WIRE, 240V SINGLE PHASE POWER, WITH GROUND, LIGHTING SYSTEM. THE CONTRACTOR SHALL WIRE EACH POLE AS SHOWN ON THE DETAIL GIVEN ON THIS SHEET.
PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH POLE TO BE WIRED BY THE CONTRACTOR.

ITEM 1001. FOUNDATION, AS PER PLAN

LIGHT POLE FOUNDATIONS SHALL BE OF AUGER TYPE EQUAL IN QUALITY, DESIGN AND PERFORMANCE TO A.B. CHANCE CO. FOUNDATION (1112-0679). (AN ADAPTER PLATE MAY BE NEEDED FOR PROPER INSTALLATION PROCEDURE, DEPENDING ON THE TYPE OF DRILL MACHINERY.) PROVIDE A GROUNDING LUG.
THE TOP 8" OF THE SCREW-IN FOUNDATION SHALL BE COATED TO MATCH THE LIGHT POLE.
THE SPECIFICATIONS ARE SUBJECT TO CHANGE AT THE DISCRETION OF THE CITY OF SUNBURY, OHIO.
PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH FOUNDATION FURNISHED AND INSTALLED BY THE CONTRACTOR.
ITEM 1001. 120/240V PAD MOUNTED LIGHTING CONTROLLER, AS PER PLAN
THE PROPOSED PAD MOUNT CONTROL SITE SHALL BE AS PER MIS-603, MIS-57 FOR METER, AND THE WIRING DETAIL PROVIDED FOR THREE WIRE, 120/240V SINGLE PHASE POWER, GROUND NEUTRAL. THE LIGHTING CONTROLLER SHALL BE DESIGNED WITH HEIGHT AS REQUIRED FOR A MAIN LUG ONLY 18 CIRCUIT LOAD CENTER WITH CIRCUIT BREAKERS AS SHOWN, AN INSULATED NEUTRAL BUS, A SEPARATE EQUIPMENT GROUND BUS, AND A TYPE 1 1449 SURGE ARRESTOR WITH LED ACTIVE STATUS INDICATOR. THE MAIN TERMS SHALL BE 30 AMPERE BUSSMAN LPN-RK OR APPROVED EQUAL. COORDINATE ALL METERING AND SERVICE REQUIREMENTS WITH CONSOLIDATED ELECTRIC. ALSO, THE CONTRACTOR SHALL INSTALL THREE #4 AWG, 600V RHW CABLES FROM THE LINE TERMINALS OF THE CONTROL PANEL TO THE POWER COMPANY'S SERVICE TRANSFORMER OR PEDESTAL, LEAVING 10' OF COILED CABLE FOR CONNECTION BY THE POWER COMPANY.
PROVIDE AN ARC FLASH HAZARD WARNING SIGN ON THE OUTSIDE FRONT DOOR OF THE ENCLOSURE IN ACCORDANCE WITH THE CURRENT NATIONAL ELECTRICAL CODE PARAGRAPH 110.16.
PROVIDE AN AVAILABLE FAULT CURRENT SIGN ON THE OUTSIDE OF THE FRONT DOOR OF THE SERVICE DISCONNECT ENCLOSURE IN ACCORDANCE WITH THE CURRENT NATIONAL ELECTRICAL CODE PARAGRAPH 110.24.
PAYMENT SHALL BE AS PER ITEM 1001 FOR EACH CONTROL SITE FURNISHED AND INSTALLED.

GROUNDING AND BONDING

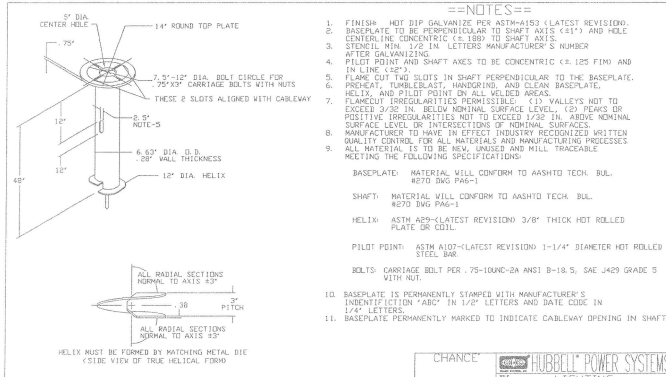
- THE REQUIREMENTS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS (CAMS) AND THE H.I. SERIES OF STANDARD CONSTRUCTION DRAWINGS ARE MODIFIED AS FOLLOWS:
- ALL METALLIC PARTS CONTAINING ELECTRICAL CONDUCTORS SHALL BE PERMANENTLY JOINED TO FORM AN EFFECTIVE GROUND FAULT CURRENT PATH BACK TO THE GROUNDING CONDUCTOR IN THE POWER SERVICE DISCONNECT SWITCH.
 - PROVIDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR IN ADDITION TO THE CONDUCTORS SPECIFIED AND BOND THE CONDUIT TO THIS GROUNDING CONDUCTOR.
 - WHEN AN EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED IN PLASTIC CONDUIT (725.05), THE INSTALLATION SHALL INCLUDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR IN ADDITION TO THE CONDUCTORS SPECIFIED. METAL PULL BOX LIDS SHALL BE BONDED BY ATTACHMENT OF THE EQUIPMENT GROUNDING CONDUCTOR TO THE FRAME DIAGONAL AS PROVIDED ON HL-30.11.
 - CONDUITS
 - THE 725.04 CONDUIT SHALL HAVE GROUNDING BUSHINGS INSTALLED AT ALL TERMINATION POINTS. THE BUSHING MATERIAL SHALL BE COMPATIBLE WITH GALVANIZED STEEL CONDUIT AND THE GROUNDING LUG MATERIAL SHALL BE COMPATIBLE FOR USE WITH COPPER WIRE. THREADED OR COMPRESSION TYPE BUSHINGS MAY BE USED.
 - THE 725.05 CONDUIT SHALL HAVE THE INSIDE AND OUTSIDE DIAMETERS OF THE CONDUIT DEBURRED AT ALL TERMINATION POINTS.
 - BOTH ENDS OF METALLIC CONDUIT SHALL BE BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
 - METALLIC CONDUIT MAY BE BONDED TO METALLIC BOXES THROUGH THE USE OF CONDUIT FITTINGS UL APPROVED FOR THIS TYPE OF CONNECTION, WITH THE BOX BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
 - WIRE FOR GROUNDING AND BONDING
 - USE INSULATED, COPPER WIRE FOR THE EQUIPMENT GROUNDING CONDUCTOR. BONDING JUMPERS IN BOXES AND ENCLOSURES MAY BE BARE OR INSULATED COPPER WIRE. WIRE SIZE SHALL BE AS FOLLOWS: THE INSULATION SHALL BE GREEN OR GREEN WITH YELLOW STRIPES, FOR 4 AWG OR LARGER, INSULATION MAY ALSO BE BLACK WITH GREEN TAPE/LABELS INSTALLED AT ALL ACCESS POINTS.
 - GROUND ROD
 - A 3/4" INCH SCHEDULE 40 PVC CONDUIT WILL BE USED IN FOUNDATIONS AND CONCRETE WALLS FOR THE GROUNDING CONDUCTOR (GROUND WIRE) BACKWAY TO THE GROUND ROD. SHOULD METALLIC CONDUIT BE USED, BOTH ENDS OF THE CONDUIT SHALL BE BONDED TO THE GROUNDING CONDUCTOR.
 - THE TYPICAL GROUNDING CONDUCTOR (GROUND WIRE) SHALL BE 4 AWG INSULATED, COPPER.
 - POWER SERVICE AND DISCONNECT SWITCH
 - AT THE POWER SERVICE LOCATION, THE GROUNDING CONDUCTOR (GROUND WIRE) FROM THE DISCONNECT SWITCH NEUTRAL (AC-) BAR TO THE GROUND ROD SHALL BE A CONTINUOUS, UNSPLICED CONDUCTOR. IF SPLICED, IT SHALL BE AN EXOTHERMIC WELD BUT SPLICE SHALL BE MADE AT THE GROUND ROD. THE SERVICE NEUTRAL SHALL ONLY BE CONNECTED TO GROUND AT THE MAIN POWER SERVICE DISCONNECT SWITCH.
 - TO PROVIDE THE COMPLETE GROUND FAULT CURRENT PATH SHALL BE CONSIDERED INCIDENTAL TO THE CONDUCTORS SUPPLIED BY THE PROJECT.



LUMINAIRE (WATTAGE), 240V, AS PER PLAN
No Scale

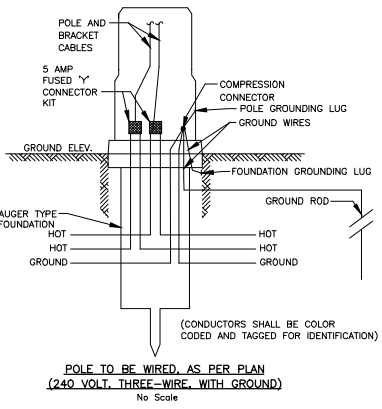
STREET LIGHTING ESTIMATE OF QUANTITIES

ITEM	COLUMBUS MIS	QUANTITIES	UNIT	DESCRIPTION
1001	54	7	EA	Pull Box, Medium Duty
1001	56	1	EA	Riser, Street Light Circuit
1001	404	1,819	CKT FT	Street Lighting Circuit Cable, As Per Plan
1001	501	14	EA	Pole to be Wired, As Per Plan
1001	57 & 603	1	EA	120/240V Pad Mounted Lighting Controller, As Per Plan
1001	700	1,799	LF	2" Conduit, Concrete Encased
1001	-	7	EA	Luminaire, 59W, 240V, With House Side Shield, As Per Plan
1001	-	1	EA	Luminaire, 59W, 240V, With House Side Shield, As Per Plan, Furnish Only
1001	-	7	EA	Luminaire, 77W, 240V, With House Side Shield, As Per Plan
1001	-	1	EA	Luminaire, 77W, 240V, With House Side Shield, As Per Plan, Furnish Only
1001	-	14	EA	Light Pole, As Per Plan
1001	-	1	EA	Light Pole, As Per Plan, Furnish Only
1001	-	14	EA	Foundation, As Per Plan

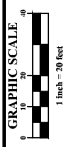


==NOTES==

- FINISH: HOT DIP GALVANIZED PER ASTM-A593 (LATEST REVISION)
- BASEPLATE TO BE PERPENDICULAR TO SHAFT AXIS (Z1) AND HOLE CENTERLINE CONCENTRIC TO SHAFT AXIS
- STENCIL MIN. 1/2" IN LETTERS MANUFACTURER'S NUMBER AFTER GALVANIZING
- PILOT POINT AND SHAFT AXES TO BE CONCENTRIC (±.125 FIM) AND IN LINE (±.01")
- FLAME CUT TWO SLOTS IN SHAFT PERPENDICULAR TO THE BASEPLATE.
- PROTECT UNDERLAYER, UNDERGIRD, AND CLEAN BASEPLATE
- FLANGES TO BE GALVANIZED PER SPECIFICATIONS (±.13) VALVEYS NOT TO EXCEED 3/32 IN. BELOW NOMINAL SURFACE LEVEL. (±) PEAKS OR POSITIVE IRREGULARITIES NOT TO EXCEED 1/32 IN. ABOVE NOMINAL SURFACE LEVEL OR INTERSECTIONS OF NOMINAL SURFACES
- MANUFACTURER TO HAVE IN EFFECT INDUSTRY RECOGNIZED WRITTEN QUALITY CONTROL FOR ALL MATERIALS AND MANUFACTURING PROCESSES
- ALL MATERIAL TO BE OF THE HIGHEST QUALITY AND MILL "WORKABLE" MEETING THE FOLLOWING SPECIFICATIONS
- BASEPLATE: MATERIAL WILL CONFORM TO AASHTO TECH. BUL. #670 B&G PAGE-1
- SHAFT: MATERIAL WILL CONFORM TO AASHTO TECH. BUL. #670 B&G PAGE-1
- HELIX: ASTM A53-LATEST REVISION) 3/8" THICK HOT ROLLED PLATE 36 CDL
- PILOT POINT: ASTM A107-LATEST REVISION) 1-1/4" DIAMETER HOT ROLLED STEEL BAR
- BOLTS: CARRIAGE BOLT PER. 75-1040-(2A) ANSI B-18.5, SAE J429 GRADE 5 1/2IN. NUT
- BASEPLATE IS PERMANENTLY STAMPED WITH MANUFACTURER'S IDENTIFICATION "ABC" IN 1/2" LETTERS AND DATE CODE IN 1/4" LETTERS
- BASEPLATE PERMANENTLY MARKED TO INDICATE CABLEWAY OPENING IN SHAFT



POLE TO BE WIRED, AS PER PLAN
(240 VOLT, THREE-WIRE, WITH GROUND)
No Scale

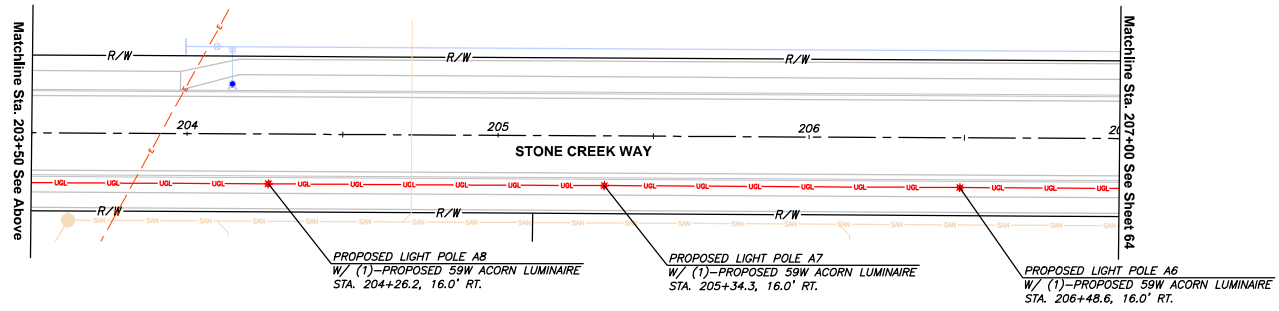
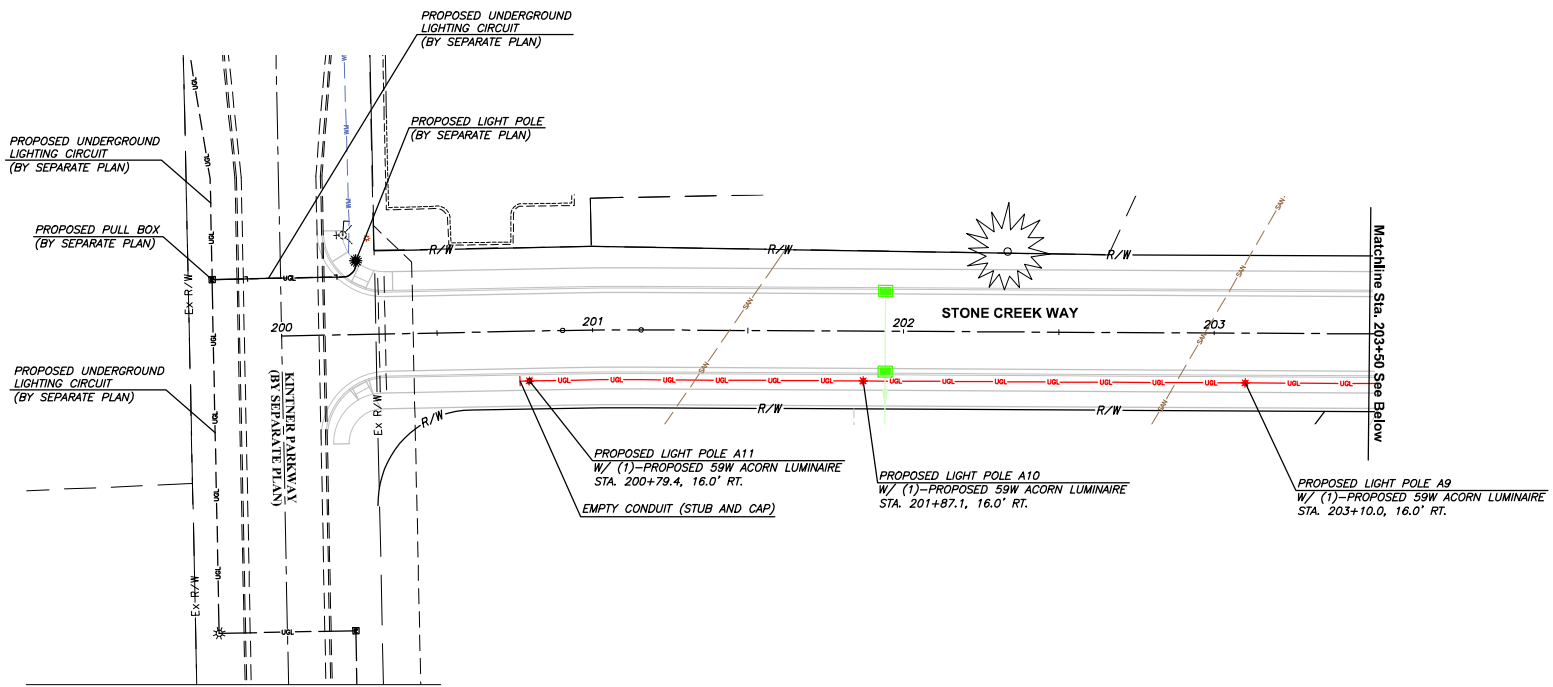


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STREET LIGHTING PLAN

CHESHIRE ROAD RELOCATION

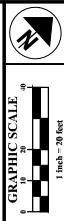
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STREET LIGHTING PLAN LEGEND

- PROPOSED 2" PVC CONDUIT, CONCRETE ENCASED
W/ (2)-#4 AWG & (1) #8 AWG GND CABLES
- PROPOSED POST TOP LIGHT POLE, 14'
AND 240V, LED LUMINAIRE (WATTAGE AS NOTED)
- PROPOSED LIGHTING PULL BOX
W/ SPLICES WHERE NOTED
- PROPOSED PAD MOUNTED
120/240V LIGHTING CONTROL CABINET

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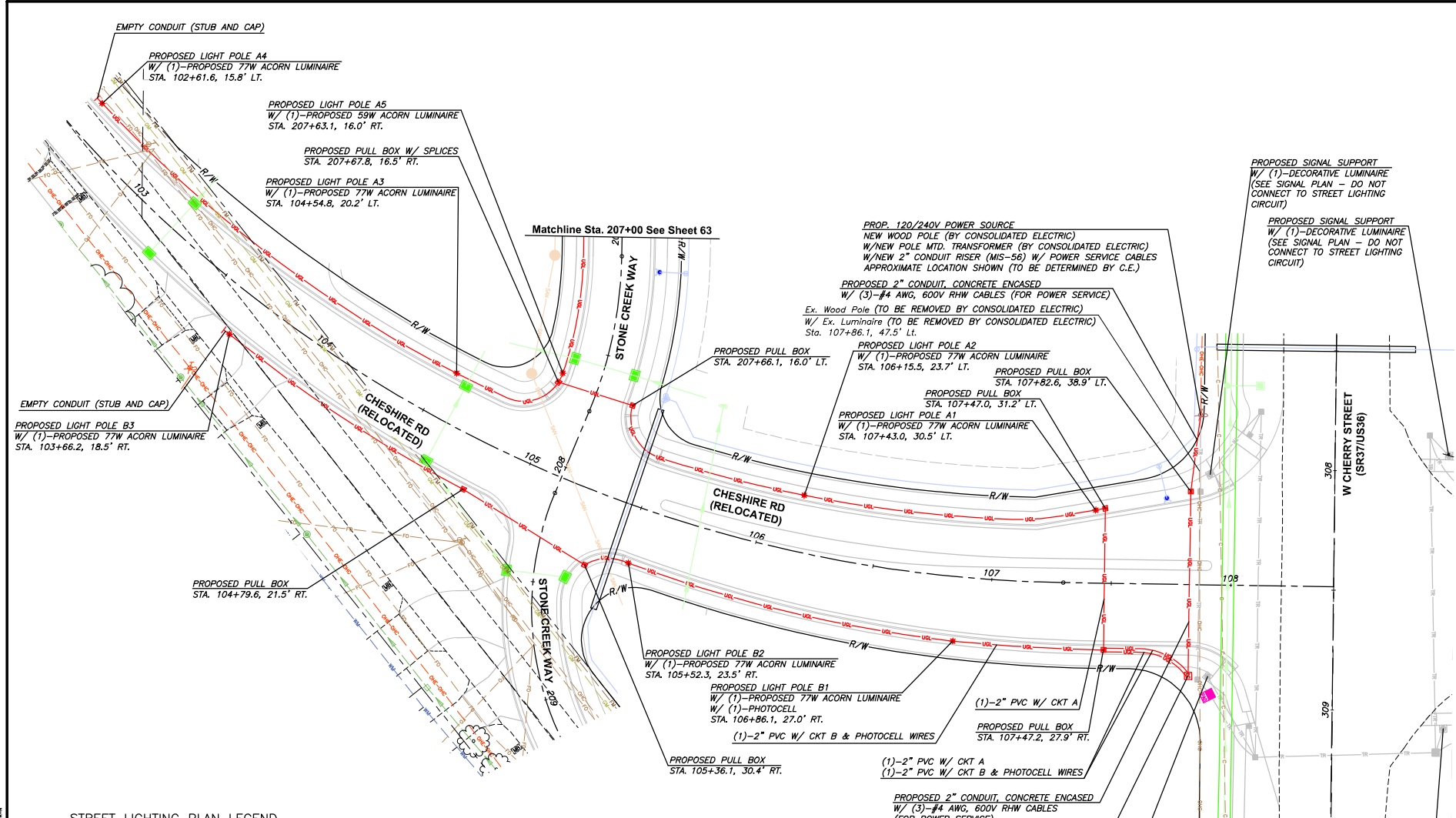


CALCULATED
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STREET LIGHTING PLAN

CHESHIRE ROAD RELOCATION

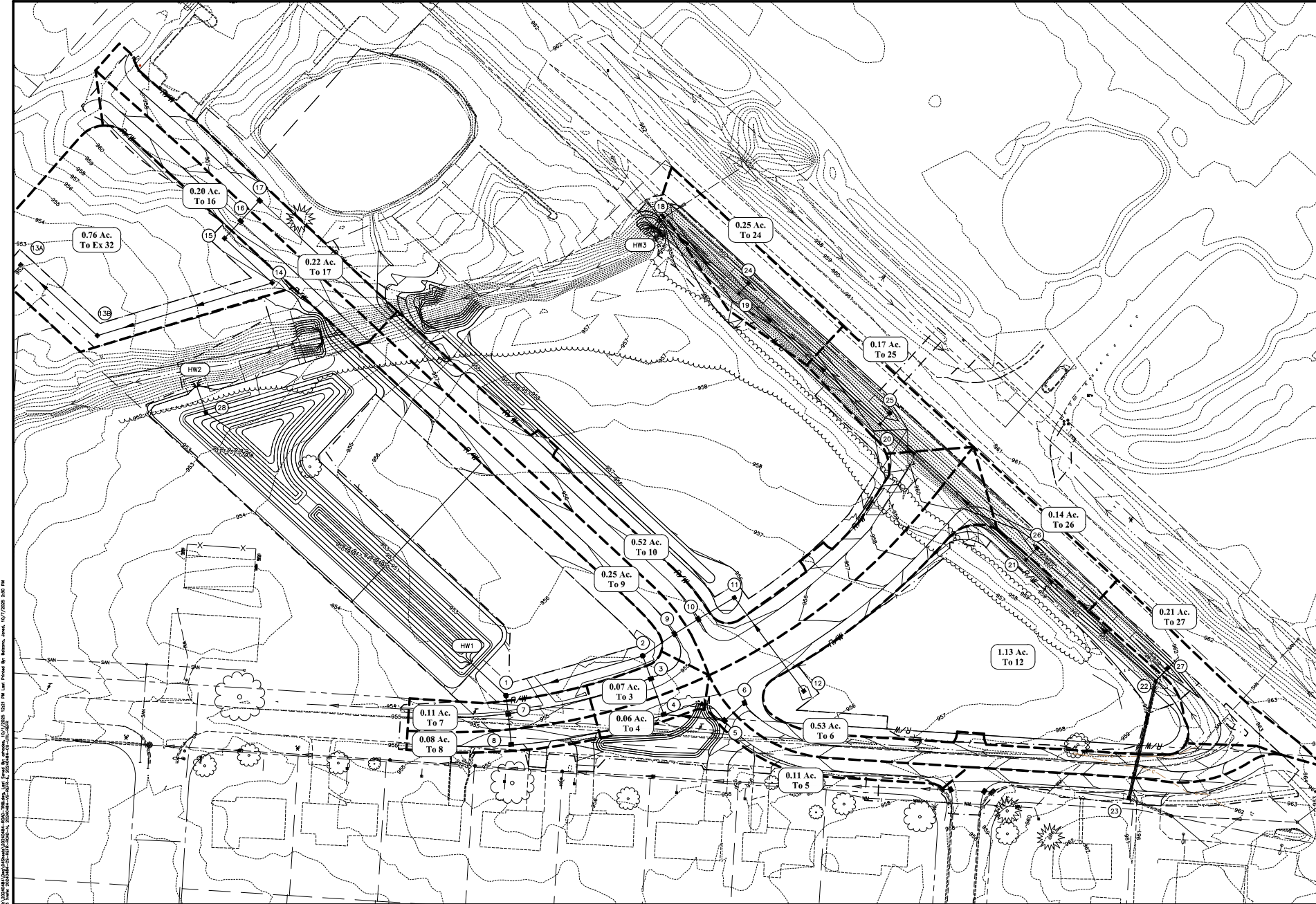
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STREET LIGHTING PLAN LEGEND

- PROPOSED 2" PVC CONDUIT, CONCRETE ENCASED
 W/ (2)-#4 AWG & (1) #8 AWG GND CABLES
- * PROPOSED POST TOP LIGHT POLE, 14'
 AND 240V, LED LUMINAIRE (WATTAGE AS NOTED)
- PROPOSED LIGHTING PULL BOX
 W/ SPLICES WHERE NOTED
- PROPOSED PAD MOUNTED
 120/240V LIGHTING CONTROL CABINET

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P:\Projects\2025\Cheshire Road Relocation\Drawings\250825_1511.dwg, 10/7/2025 15:11 PM, User: jw, Plotter: HP DesignJet 5000, 10/7/2025 3:05 PM
 P:\Projects\2025\Cheshire Road Relocation\Drawings\250825_1511.dwg, 10/7/2025 15:11 PM, User: jw, Plotter: HP DesignJet 5000, 10/7/2025 3:05 PM

GRAPHIC SCALE
CALCULATED JWB CHECKED MUS
TRIBUTARY AREA MAP
CHESHIRE ROAD RELOCATION
65 65